



## MONROE PLANNING COMMISSION

<b>SUBJECT:</b>	<b><i>DISCUSSION - North Kelsey Design Guidelines Amendment, Code Text Amendments regarding Multifamily Residential Uses in the North Kelsey/Tjerne Place Planning Area, and Zoning Map Amendments.</i></b>
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<b>DATE:</b>	<b>DEPT:</b>	<b>CONTACT:</b>	<b>PRESENTER:</b>	<b>ITEM:</b>
10/12/2020	Community Development	Anita Marrero	Anita Marrero	<b>New Business # 1</b>

**Discussion:** 10/12/2020

**Public Hearing:**

**Attachments:**

1. North Kelsey Design Guidelines
2. DRAFT North Kelsey Design Guidelines
3. DRAFT Code Amendment
4. Zoning Map and Subarea Map
5. SEPA Checklist
6. Traffic Study

**REQUESTED ACTION:** None at this time. Informational presentation and discussion only.

### POLICY CONSIDERATIONS

On April 10, 2018, the City Council directed the Planning Commission to evaluate amending the Monroe Municipal Code (MMC) to allow multi-family residential in the North Kelsey Planning Area. The code amendments were included in the UDR update. Further code amendments are required as well as amendments to the North Kelsey Design Guidelines and Zoning Map for consistency with the current code.

### DESCRIPTION/BACKGROUND

In 2005, the City purchased approximately twenty-three acres of land from Snohomish County north of US 2. The City master planned the property, together with other City-owned land in the area, with the intent of developing retail, office space, and a public “village green” for community gatherings. The original North Kelsey Development Plan was created with public input and approved in 2003 by Ordinance No. 015/2003, amended in 2007 by Ordinance No. 024/2007, and amended again in 2012 by Ordinance No. 027/2012. The City conducted a Request for Proposal process in 2005 to develop the property. The first property was sold to Lowes in (December 2006) just before the recession in 2008. Two additional properties were sold to Providence Medical Center (October 2012) and Walmart (September 2013).

The remaining ten parcels encompass approximately eleven acres and are located on Tjerne Place between Chain Lake Road on the east and Kelsey Street on the west. One parcel (Tract 999) is located on the former Monroe landfill west of Kelsey Street between the Galaxy Movie Theater and Fred Myer. A second remnant parcel is located between Walmart and Chain Lake Road.

The City has not sold any parcels since 2013. The market for retail and office space has fundamentally changed since the recession and the growing dominance of on-line sales and e-commerce retailers such as Amazon. In February 2018, the City Council selected a new broker

team to market and sell the property. The current team is working to update the site plan to reflect changes in the retail and housing market.

The proposed code amendments in 2018 were included in the UDR update which provided for a combination of horizontal and vertical mixed-use, multifamily housing, retail space, and a public “village green” within the North Kelsey/Tjerne Place Planning Area. The revisions to the City’s development regulations included allowing for residential development within the permitted land uses, increasing maximum building heights, and revising minimum parking standards to incorporate five stories of living space above parking in the North Kelsey/Tjerne Place Planning Area.

The current proposed amendments include amendments to the North Kelsey Design Guidelines to address medium box stores, service based industries like childcare, and multifamily development; amendments to the zoning map as the current zoning map inadvertently left out the original North Kelsey Planning Area and the North Kelsey Planned Development Area; and code text amendments to delete references to mixed-use development in the North Kelsey/Tjerne Place overlay district.

**FISCAL IMPACT**

N/A

**TIME CONSTRAINTS**

N/A

# **North Kelsey Development Plan Design Guidelines Update 2012**

## **APPENDIX 1: Design Guidelines**

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**December 6, 2012**

*Approved Ord. 008/2003  
Amended Ord. 024/2007  
Amended Ord. 027/2012*

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# CHAPTER 1:

## Introduction

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### A. Purpose

These design guidelines are a critical regulatory tool in implementing the design-related goals and objectives for the North Kelsey Planning Area and the North Kelsey Planned Development Area.

### B. Intent

These guidelines are directed to creating a development within the North Kelsey planning area that:

- Provides a visible and accessible commercial district for the City of Monroe.
- Enhances downtown circulation for pedestrians and vehicles.
- Connects and integrates other downtown activities.
- Accommodates retail, office, and service development of various sizes and character as is appropriate for Monroe's small town character with design quality of the highest caliber.
- Enhances the town's identity as a regional attraction.

### C. Application of Design Guidelines

The design guidelines apply to all new construction in the North Kelsey Planning Area (Figure 1). The sole exception to the guidelines involves interior remodels. The guidelines are intended to supplement other requirements in the Monroe Municipal Code. Where the guidelines and zoning ordinance standards conflict, the City shall determine which regulation applies.

All properties that are outside the development area, but within the North Kelsey Planning Area are subject to Chapters 5 through 7 of these Design Guidelines. This includes the privately owned parcels within the outlined area.

The specific planned development area is comprised of properties as reflected in Figure 1, Applicable Properties.

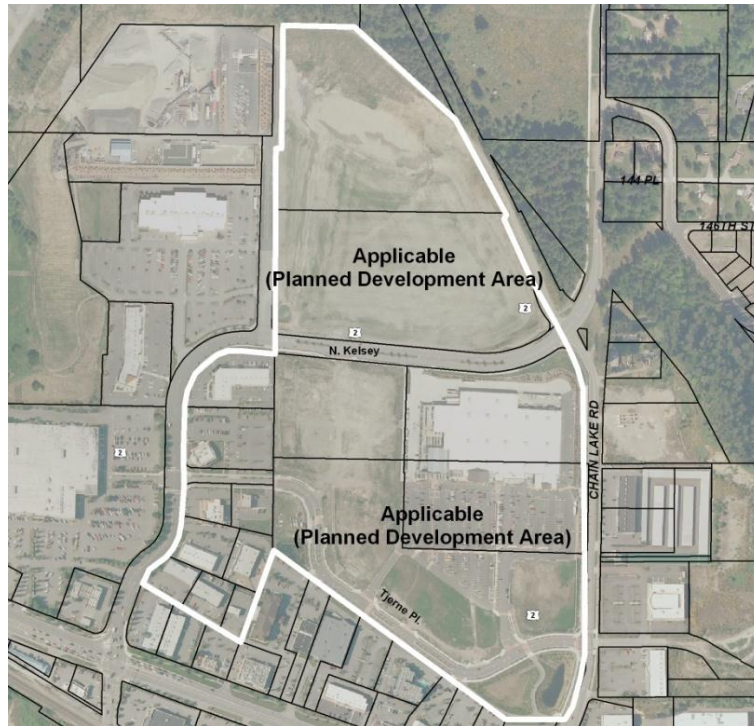


Figure 1: Applicable Properties.

## D. Interpreting the Design Guidelines

The City retains full authority to determine whether or not a proposal meets these guidelines. Within the guidelines, certain words are used to indicate the relative importance and priority the City places upon the particular guideline. The words “shall,” “must,” and “is/are required” mean that the development proposal must comply with the guideline unless the City finds that:

- The guideline or requirement is not applicable or appropriate in the particular instance, or;
- The development proposal meets the intent of the guidelines in some other manner.

The words “is/are encouraged” mean that the action or characteristic is allowed and should be viewed as a positive element in the City’s review.

The project proponent may submit proposals that he/she feels meet the intent of the guidelines but not necessarily the specifics of one or more guidelines. In this case, the City will determine if the intent of the guideline has been met.

## E. Review Process

The Design Guidelines will be used, along with other City ordinances and regulations, for the City’s review of development proposals within the North Kelsey Development Area.

## CHAPTER 2: Site Configuration

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The development must address the following principles: The proposal shall indicate how the development will connect to adjacent properties/parcels. The means of pedestrian and vehicular circulation, as well as building and entry orientation, must be as approved by the City in accordance with the Design Guidelines as applied to the entire area of applicability as stated in Chapter 1C.

1. Connects the following activities with an integrated pedestrian network:

- Southeast: Chain Lake Road sidewalk.
- North: North Kelsey Street.
- West: North Kelsey Street near the entry to the Fred Meyer store.
- South: Current and future development along Tjerne Place.

Gateway features and safe walking connections must be provided at these points.

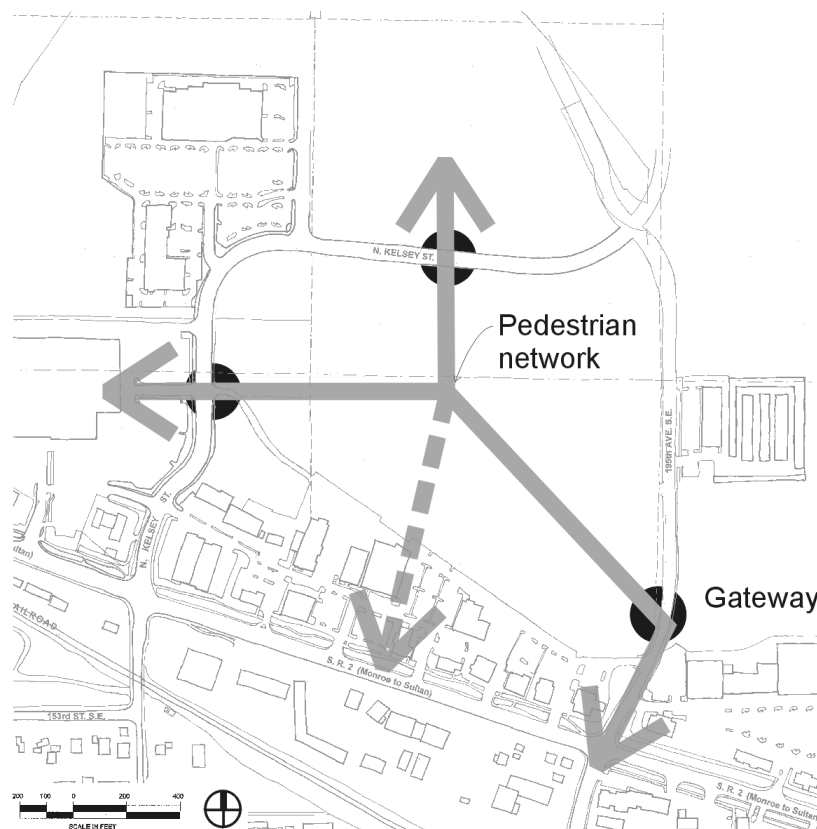


Figure 2: Key pedestrian connections.

2. Locates and treats large buildings to reduce their perceived scale to fit with neighboring structures and present an inviting, human scaled, pedestrian oriented character to the public. (See Architectural/Building Design guidelines, Chapter 5.)
3. Includes Tjerne Road to connect North Kelsey Street and Chain Lake Road, creating a loop system around the south parcel. Parking for future development should be accessed from this loop system (See Parking Area guidelines in Chapter 3 and Circulation Guidelines, Chapter 4). Opportunities for sharing access and parking between current and future uses, including the theater complex, should be explored.

**The project proponent must demonstrate that the overall site layout and circulation system accomplishes these goals to the City's satisfaction.**

# CHAPTER 3:

## Site Planning

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### A. Public Open Space

#### Intent:

- Provide an attractive pedestrian environment.
- Provide outdoor spaces with pedestrian amenities.

#### Pedestrian-Areas

#### Guidelines:

1. Pedestrian-areas are encouraged along the pedestrian connections and near key building entries. They can be small to large widening of walking space, landscaped areas, areas for outdoor dining, or small play areas.
2. Pedestrian amenities shall be provided such as seating, plants, drinking fountains, distinctive paving, artwork, and such focal points as sculpture or water feature, should be provided.
3. Lighting fixtures should be approximately 10-15 feet above the surface and may be building mounted. The overall lighting in pedestrian areas should be at least 2 foot-candles, without any “dark spots” that could cause security problems. Ambient light from under canopies or storefronts may be included in the lighting calculations.
4. The spaces must have visual and pedestrian access (including barrier-free access) to abutting structures and public streets or pathways.
5. Landscaping that does not act as a visual barrier is encouraged
6. Buildings abutting pedestrian-oriented space must have pedestrian-oriented facades (see Building Orientation guidelines below).
7. See Chapter 6 Landscape Design for applicable landscaping guidelines.

### B. Building Orientation

#### Intent:

- Provide an attractive pedestrian environment.
- Enhance the character of the streetscapes within and surrounding the area.
- Provide attractive building facades adjacent to parking lots.

## **General:**

Buildings and ground floor businesses fronting pedestrian areas should be oriented towards these areas. Since these and other buildings also front onto parking lots or streets, buildings and businesses are strongly encouraged to provide secondary building entrances. These are referred to as “Secondary Pedestrian Facades.” Side and rear walls of new buildings in the planning area will also be visible and therefore must be designed and/or screened to provide an attractive streetscape. These are referred to as “Side or Rear Facades.” Locations, guidelines and standards for, Secondary Pedestrian Facades, and Side or Rear Facades are detailed below.

### **1. Secondary Pedestrian Façade:**

- a. Where: Building facades adjacent to parking lots, pedestrian pathways, or streets, as designated by the City.
- b. Weather protection over the building entry and covering at least 50 percent of the overall façade is required.
- c. Storefront windows over at least 50 percent of the facades on the ground floor between the heights of 2 to 8 feet above the ground are required.
- d. Building entries along secondary pedestrian facades should utilize pedestrian-oriented lighting and/or decorative façade details. (See Building Elements and Details, Chapter 5).
- e. Blank walls must be treated in one or more of the following ways:
  - Planters or trellises with vines.
  - Landscaping that covers 30 percent of wall area within three years of planting.
  - Special materials (e.g., decorative patterned masonry).
  - Display windows.
  - Other treatment approved by the City.

### **2. Side and Rear Facades:**

- a. Building facades building/business entries, or highly visible locations.
- b. What: While pedestrian building/business entries are not required here, they may be encouraged depending on specific site characteristics. Service elements may be located here (see Building Equipment and Service Area guidelines, Chapter 5). Facades shall be treated in two or more of the following ways (Figure 4):
  - Planters or trellises with vines.
  - Landscaping that covers 30 percent of wall area within three years of planting.
  - Special materials (e.g., decorative patterned masonry).
  - Display windows.
  - Other treatment approved by the City.

- c. Designated side and rear facades located along public streets should be set back at least 20 feet from the sidewalk to accommodate stormwater treatment methods and heavy landscaping.
- d. Visible building façades should be articulated per “Vertical Articulation” guidelines in Chapter 5 under Human/Pedestrian Scale.
- e. Creative use of building materials such as concrete and concrete masonry units is encouraged.

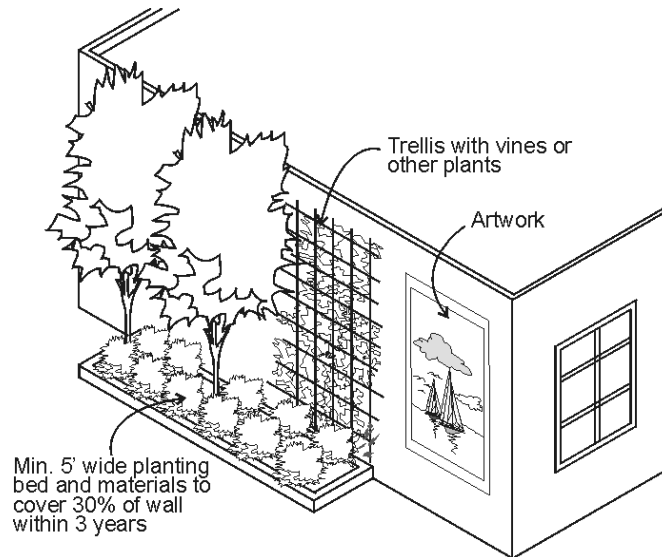


Figure 3: Blank wall treatments.

## C. Parking Areas

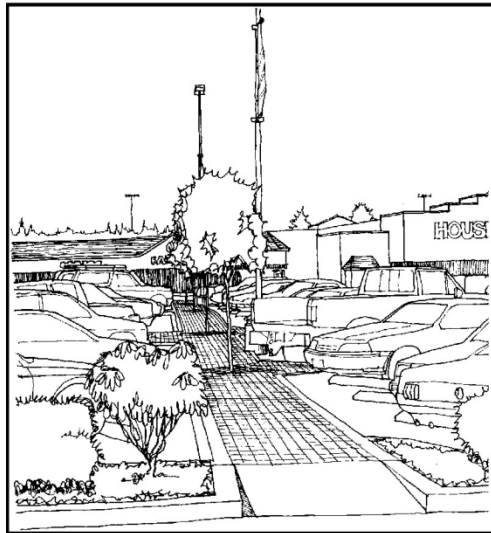
### Intent:

- To provide convenient parking areas that encourage people to leave their cars and walk throughout the North Kelsey Planning Area.
- To provide more flexibility in the design of the development by relaxing existing City parking standards.
- To provide parking areas that do not diminish pedestrian and visual qualities of the site.
- To maintain the built street edge through effective screening of all parking lots.
- To minimize the impacts of driveways.



**Guidelines:**

1. Parking areas shall conform to the requirement of MMC, Chapter 18.86 unless otherwise noted in these guidelines. This encompasses dimensional requirements, design, access, loading areas, number of parking spaces, parking area landscaping, and other parking-related requirements.
2. Parking requirements for retail uses shall be relaxed to 1 space per 250 square feet of gross floor area. The City may consider special provisions for joint use of parking when two activities are less likely to occur simultaneously (e.g. office uses and entertainment facilities).
3. The landscaped buffer between the sidewalk and the parking area along Chain Lake Road must be expanded to at least 10 feet in width using either Type II or Type III Landscaping standards (MMC, Section 18.78.030) subject to City approval.
4. Pathways through parking lots should be provided. Pathways and crosswalks should be provided along every fourth parking isle or at intervals of less than 150 feet (See Figure 5). Pathways through parking areas should be separated from vehicle parking and travel lanes by use of contrasting surface materials, which may be raised above the level of the vehicular surface. Parking area pathways should be at least 4 feet in width.



*Figure 4: Parking pathway example.*

5. Structured parking is encouraged provided the building meets the guidelines of Chapter 5.

## D. Street Corners/Highly Visible Locations

### Intent:

- To enhance the appearance of highly visible locations.
- To enhance the pedestrian environment.
- To establish a design identity for the North Kelsey Planning Area.

### Guidelines:

The guidelines below highlight desirable design treatments (options noted below) for six specific street corners and/or highly visible locations. All proposals for sites should include at least one of the design treatments described below. **EXCEPTION:** Applicants may propose other design treatments for these sites if they can demonstrate successfully that the proposed treatment meets the intent of the guidelines.

1. Street Corner/Highly Visible Location Design Treatment Methods (also refer to Pedestrian Orientation guidelines, Chapter 3):
  - a. Locate a building towards the street corner (within 15 feet of corner property line). Building facades located here are encouraged to include a special element, such as a raised roofline, towers, or an extended parapet, along the most visible views of the structure (See Figure 6).
  - b. Provide a pedestrian walkway and/or plaza space at the corner leading directly to a building entry or shopping plaza space. May be appropriate in conjunction with a Monument Site Entry Sign.
  - c. Install substantial landscaping (at least 200 square feet of ground surface area with trees, shrubs, and or ground cover. May be appropriate in conjunction with a Monument Site Entry Sign.



*Figure 5: Street corner example: This building celebrates its corner location by including a corner entry, pedestrian space, weather protection, parapet, and special signage.*

## 2. Specific Sites (See Figure 7):

- a. SR-2/Chain Lake Road (northwest corner): Method “c” is the first preference. High priority site for a Monument Site Entry Sign.
- b. Chain Lake Road/Tjerne Place (both westerly corners): Method “a” or “b” is preferred for the northwest corner; Method “a” is preferred for the southwest corner, with Method “c” as a second preference.
- c. Chain Lake Road/North Kelsey Street (both westerly corners): Method “c” is preferred for both the northwest and southwest corners. High priority site for a Monument Site Entry Sign (see Signage guidelines, Chapter 7).
- d. North Kelsey Street (at key pedestrian crossing): Method “a” is preferred for all four corners.
- e. North Kelsey Street/Tjerne Place (easterly corners): Method “a” is the preferred treatment of both corners; Methods “b” and “c” are acceptable.

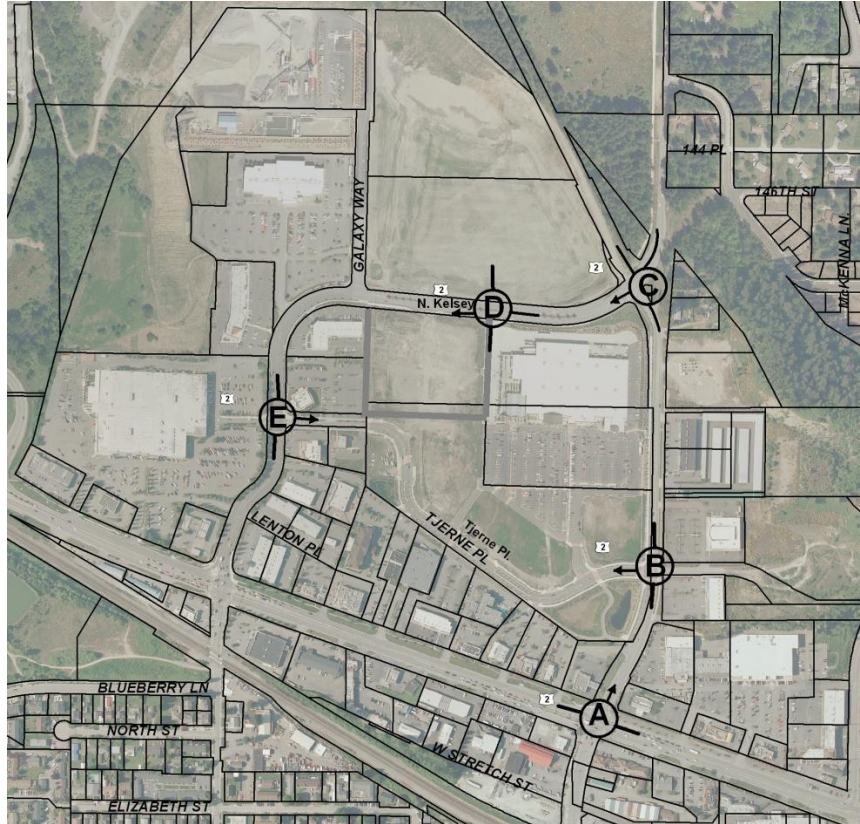


Figure 6: Highly visible locations.

# CHAPTER 4:

## Circulation

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### A. Sidewalks and Pathways

*NOTE: Sidewalks refer to concrete pedestrian routes adjacent to public rights-of-way. Pathways refer to all other pedestrian routes.*

#### Intent:

- Provide a safe environment for pedestrians to move throughout the North Kelsey planning area and separation of pedestrian and vehicular traffic.
- Create a varied and rich environment to encourage people to explore the area on foot.

#### Guidelines:

1. All public open spaces, walkways, and sidewalks shall meet ADA standards.
2. Sidewalks should be separated from the roadway by planting strips with street trees wherever possible. Planting strips should generally be at least 5 feet in width and include evergreen shrubs no more than 4 feet in height and/or ground cover in accordance with the City of Monroe Landscape Standards (MMC Chapter 18.78), and canopy-type broadleaf trees placed an average of 25 feet on center. EXCEPTIONS: Where space is limited, planting strips less than 5 feet in width may be permitted by the City; Street trees placed in tree grates may be more desirable than planting strips in key pedestrian areas.
3. Acceptable sidewalk widths may range from 4 to 12 feet depending on adjacent uses and anticipated pedestrian activity. Sidewalks along major connector routes such as North Kelsey Street or Chain Lake Road should be at least 8 feet in width to accommodate two couples passing each other.
4. Pedestrian crosswalks shall be provided at all intersections. These shall be indicated with distinctive paving.
5. The addition of texture to the ground plane of key sidewalks and pathways with unit pavers, bricks, tiles, or public artwork is encouraged.
6. Pathways that provide key access to other key sites are termed “Primary Pathways.” Primary pathway surfaces should be at least 15 feet in width to accommodate fire apparatus access and groups of people.
7. Other pathways are termed “Secondary Pathways.” Secondary Pathways may vary in width according to intended function and expected use (subject to City

8. approval). Where secondary pathways are located within corridors between structures, such corridors should be at least 12 feet in width.
9. Pedestrian amenities, including landscaping and seasonal flowers, benches, lighting, and/or artwork, shall be provided along Secondary Pathways to create visual interest (see Landscaping guidelines in Chapter 6).
10. Safe pathways to all uses and buildings and around and through parking areas are required (see Parking Area guidelines, Chapter 3).

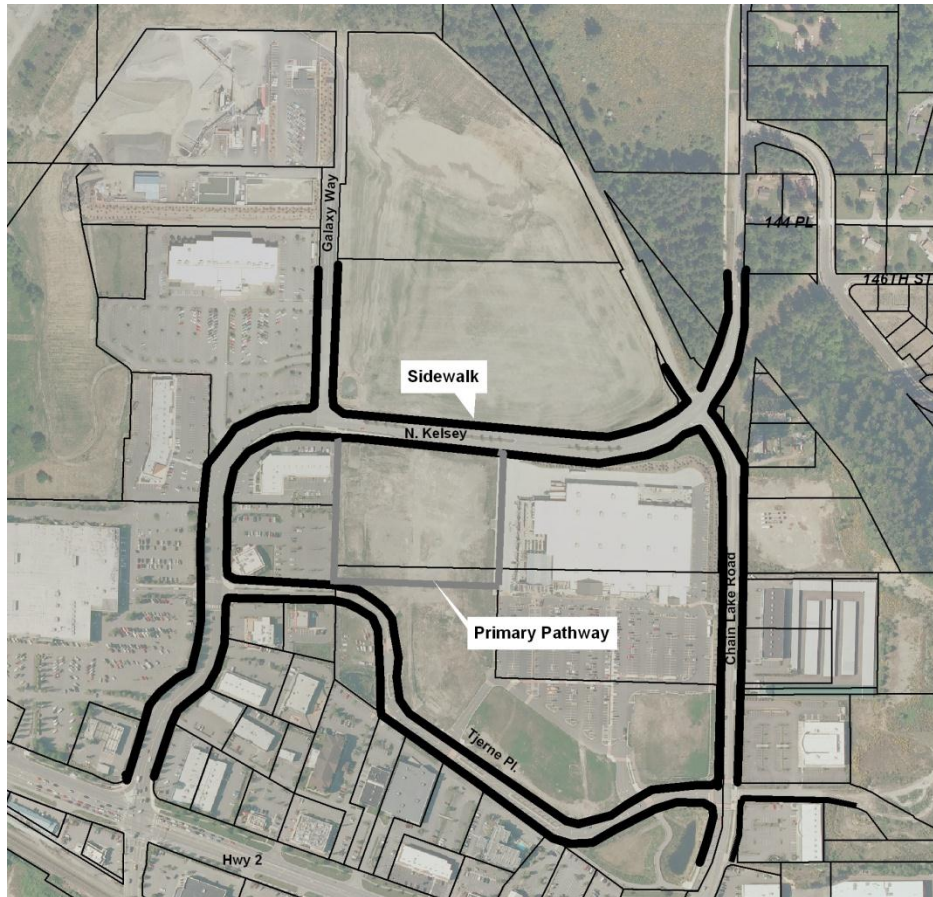


Figure 7: Example pedestrian network for the North Kelsey Planned Development Area.

## B. Bicycle Circulation and Amenities

### Intent:

- Provide safe and efficient bicycle access to and within the North Kelsey Planning Area.
- Promote bicycling as an alternative method of transportation.

**Guidelines:**

1. Safe bicycle access should be provided within each public right-of-way developed within the North Kelsey planning area. The City will consider the following options:
  - a. Bike Lanes. Standard bike lanes are 5 feet in width. This is the preferred option for Chain Lake Road (where there is sufficient right-of-way width) since bicycle lanes would connect with planned bicycle lanes north of the site.
  - b. Wide Curb Lanes. This involves 14-foot travel lanes rather than the standard 11- or 12-foot lanes so cyclists can safely share the road with vehicles. Although such wide curb lanes are often striped, they are not signed or officially designated as bike lanes. With limited space, this is often the most effective way to provide safe bicycle access.
  - c. Multi-Use Pathway. This combines bicycle and pedestrian access on an asphalt pathway separated from the roadway. Ideally, such a multi-purpose pathway should be 12 to 14 feet in width. Where space and use are expected to be limited, an 8-foot wide pathway (with center striping) may be acceptable. Pathway design should ensure adequate site distance.
2. Special care should be exercised on how either of these bicycle facilities transition to existing and planned off-site roadways – particularly Chain Lake Road and North Kelsey Street towards SR-2. Where necessary, provide signage to note safest bicycle access routes.

# CHAPTER 5:

## Architectural/Building Design

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### A. Architectural Concept

#### Intent:

- Create, through the architectural, landscape, open space, and gateway elements, an identity that reflects Monroe's vernacular architectural character.
- Provide a high-quality image with well-designed and detailed buildings, minimization of corporate identity elements (stock buildings and signs), and an emphasis on subtlety and refinement rather than on flashy or trendy design themes.
- Create varied, non-homogenous buildings within the planned development area that give the sense of natural evolution over time rather than a result of a single, one-step development—and to emphasize the fact that the building elements can naturally evolve and change over time.

#### Guidelines:

1. The buildings proposed for the North Kelsey development area should be based on a comprehensive architectural concept that achieves the intent statements above. Specifically, the design of the specific buildings should address:
  - Pedestrian interest, comfort and connections throughout the area.
  - Building size and massing elements at human scale.
  - Perceived massing of the building relative to nearby structures, pedestrian areas, and landscape elements.
  - Monroe's architectural and cultural setting.
  - The variety of sequential experiences and design characters within the site.

While the individual design guidelines in this section address some of these issues specifically, the intent of this guideline is to encourage the designers to consider how the various aspects of the design work together. Applicants should be prepared to demonstrate how the proposed buildings respond to the intent statements. The City will review applicants' proposals and determine whether or not they meet the intent.

### B. Human/Pedestrian Scale

#### Intent:

- Create a development with human scaled appearance and informal architectural character.



- To architecturally address large buildings to ensure that they do not dominate.
- Provide interesting and sheltering pedestrian-oriented facades.

## Guidelines

1. **Building Height:** Commercial/office buildings should be 1 to 3 stories high, with a maximum height of 35 feet. The City will consider higher building heights if the applicant can demonstrate consistency with overall design guidelines intent. Special features such as towers or clerestories may be taller, if approved by the City.

Sunlight should be considered within the planned development area with regard to the height of buildings adjacent to. Generally, buildings on the south side of these spaces should be sized to allow direct year-round sunlight on south-facing structures.

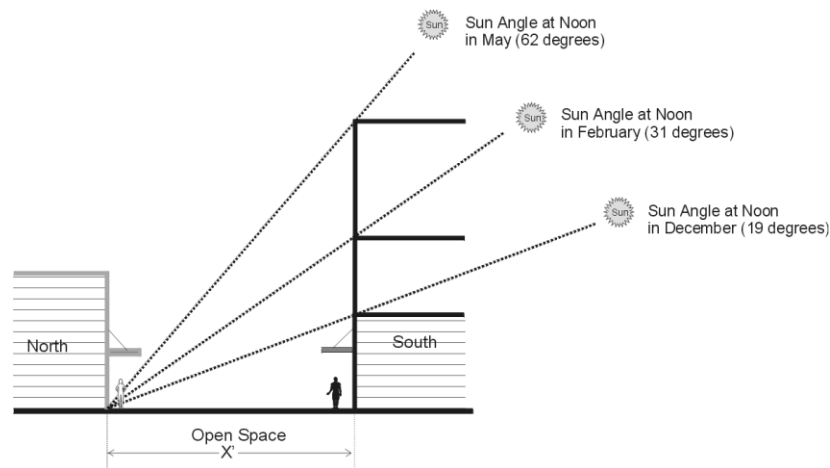
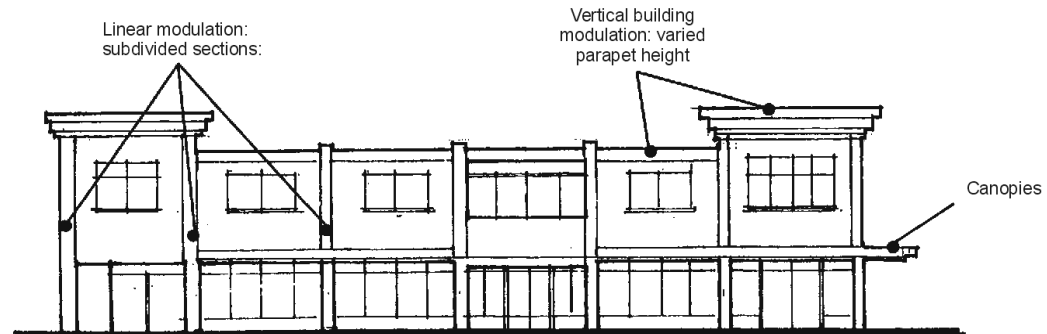


Figure 8: Sunlight should be considered in the height and design of structures adjacent to major spaces.

2. **Vertical Articulation:** In order to prevent long stretches of monotonous façade, buildings with visible facades over 100 feet in length as measured parallel to a roadway, parking area, pedestrian connection, or pedestrian area should be vertically articulated into sections averaging not more than 50 feet along the façade at regular intervals. Articulation may be accomplished in several ways, including:
  - Modulation—the stepping back or projection of a portion of the façade.
  - Including significant building elements such as balconies, porches, canopies, towers, or entry areas that visually break up the façade.
  - Building focal points that include, for example, distinctive entry features.
  - Changing the roofline.
  - Changing materials.

- Landscaping.
- Using other methods acceptable to the City.



*Figure 9: Building articulation: Varied parapet and recessed entries.*

## C. Architectural Elements

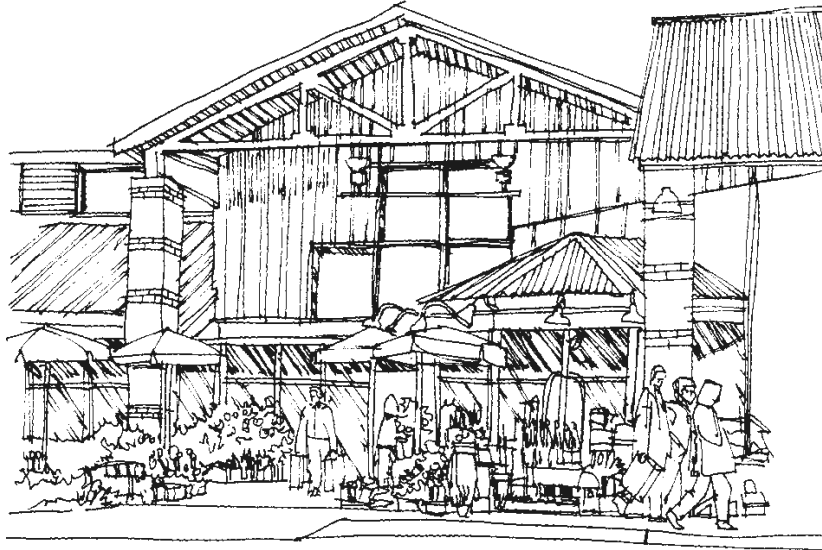
### Intent:

- Create an intimately scaled, pedestrian friendly and informal architectural character.
- Reflect Monroe's vernacular architectural character (excluding the post-War highway strip development).
- Enhance the quality of both individual buildings and the North Kelsey Planning Area streetscape as a whole.
- Encourage use of quality building materials with a low life cycle cost.
- Create design unity, a sense of place, and community identity.
- Reduce the visibility of unsightly service and utility elements from view while providing efficient service and equipment areas.

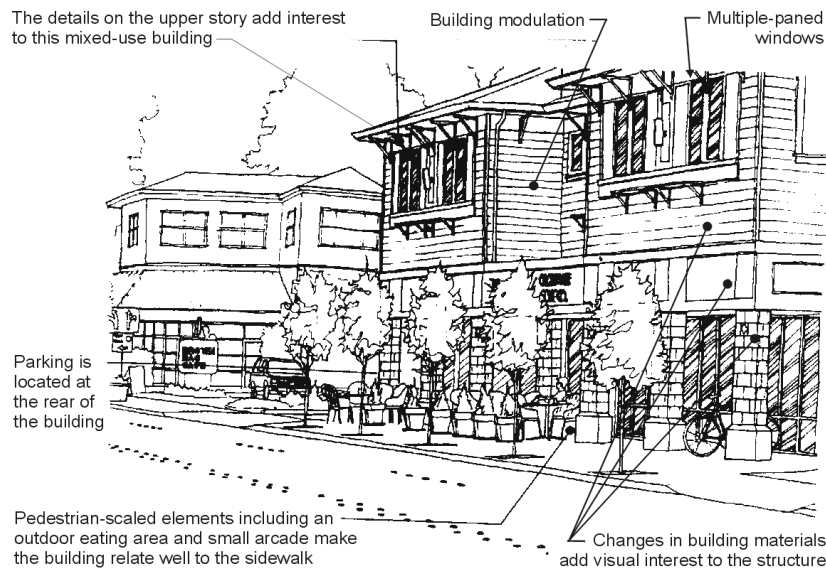
### 1. Roofs

#### Guidelines:

1. Roof designs should provide scale-reducing elements within the North Kelsey planned development area. It is recommended that buildings have a variety of roof slopes, details, materials, and configurations.
2. All flat roofs shall be architecturally treated or articulated with a parapet wall combined with ornamental molding, entablature, frieze, cornice, or other architectural roofline detail visible from the ground level. Parapets and articulated cornice lines should not appear as applied elements.
3. Roof-mounted mechanical equipment (HVAC) must be screened from view.



*Figure 10: The gabled roof over the building entry together with pedestrian-scaled elements and a variety of building materials provides visual interest to this largely square, flat-roofed building.*



*Figure 11: Examples of building details.*

## 2. Building Elements and Details

### Guideline:

1. All building facades shall incorporate a substantive use of building elements, such as those from the list that follows, as approved by the City. "Substantive" in this case means a significant contribution to the form and character of the building. Note that "decorative" means that the feature exhibits special

craftsmanship or distinctive design that adds visual interest and/or unique character. Suggested building elements include:

- Articulated building elements through treatment of windows, doors, entries, and corners with special trim, molding, or glazing.
- Permanent pedestrian weather protection (building canopy).
- Decorative building materials, such as tile and metal work.
- Enhanced or articulated building entrances (recessed or covered).
- Pergolas, arcades, porches, decks, or bay windows.
- Balconies in upper stories.
- Address numbers legible to the public from the street or pathway fronting the property or building.
- Multiple-paned window fenestration (windows with several panes separated by mullions).
- Windows. All windows should either have a vertical orientation (e.g., be longer in the vertical dimension than in the width) or be square in order to qualify as special elements.

### **3. Exterior Materials**

#### **Guidelines:**

1. Use durable and high-quality materials. Shiny or highly reflective materials are not allowed. Materials should be those of typical use in the Northwest, including:
  - Bevel or lap siding.
  - Rock, stone, and brick material.
  - Architectural shake-style roofing.
  - Metal roofs with standing seams.
2. If sheet materials, such as composite fiber products or metal siding, are used as a siding material over more than 25 percent of a building's façade, use material with a matted finish in a muted color as specified in Color guidelines below. Include the following elements:
  - Visible window and door trim painted or finished in a complementary color.
  - Corner and edge trim that covers exposed edges of the siding material.
3. If concrete blocks (concrete masonry units or "cinder blocks") are used for walls that are visible from a public street or park, use one or more of the following architectural treatments:
  - Use of textured blocks with surfaces such as split-face or grooved.
  - Use of colored mortar.
  - Use of other masonry types, such as brick, glass block, or tile, in conjunction with concrete blocks.

- Other treatment methods approved by the City.
  - The applicant shall provide the City with samples of the material, proposed detail connections and a list of other project examples in the Puget Sound region that have used this application.
4. Do not use the following materials in visible locations unless an exception is granted by the City:
    - Mirrored glass.
    - Corrugated fiberglass.
    - Chain-link fencing (with or without slats).
    - Synthetic materials with reflective surfaces, including galvanized steel and glossy vinyl siding.
    - Other treatment methods approved by the City.
  5. Paint all vents, gutters, downspouts, flashing, and electrical conduits to match the color of the adjacent surface unless they are being used expressly as a trim or accent element, or if the surface is made of an unpainted material such as brick.
  6. Provide approved address numbers so that they are legible to the public from the street fronting the property.

## **4. Colors**

### **Guidelines:**

1. Submit a color palette.
2. Muted colors are encouraged for the background color of most buildings. A darker background color will allow the effective use of lighter colors for trim – where the highlights will show up better.
3. Bright colors should generally be reserved for accents. Doors or special features may be painted a bright accent color.
4. Bright luminescent or day-glow colors are not allowed.

## **5. Building Equipment and Service Areas**

### **Guidelines:**

1. Building service elements and utility equipment should be contained within the building envelope, screened from public view, or on roofs where not visible to the public.
2. All on-site service areas, loading zones, outdoor storage areas (except outdoor retail sales areas under 100 square feet in occupied area), waste storage, disposal facilities, transformer and utility vaults, and similar activities shall be located in an area not visible from a public street, pedestrian connection, or open space. If

this is not possible, then the service area, loading zone, storage area, or utility area must be screened from public view. Acceptable screening includes:

- A masonry or wood enclosure incorporated into a building wall.
- A solid hedge or other screening as approved by the City.

(Note: Visible chain link fencing with or without slats is not permitted.)

3. Service or utility areas or enclosures shall not be located in or be visible from pedestrian areas.

# CHAPTER 6:

## Landscape Design

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### **Intent:**

- Achieve a high quality landscape that features a variety of plant materials.
- Utilize landscape materials to strengthen and unify the planning area's design identity.
- Select plant materials that are relatively hardy and require minimal maintenance.
- Add color, texture, and interest to the center.
- Screen high-impact uses.

## **A. Landscape Plan Concept**

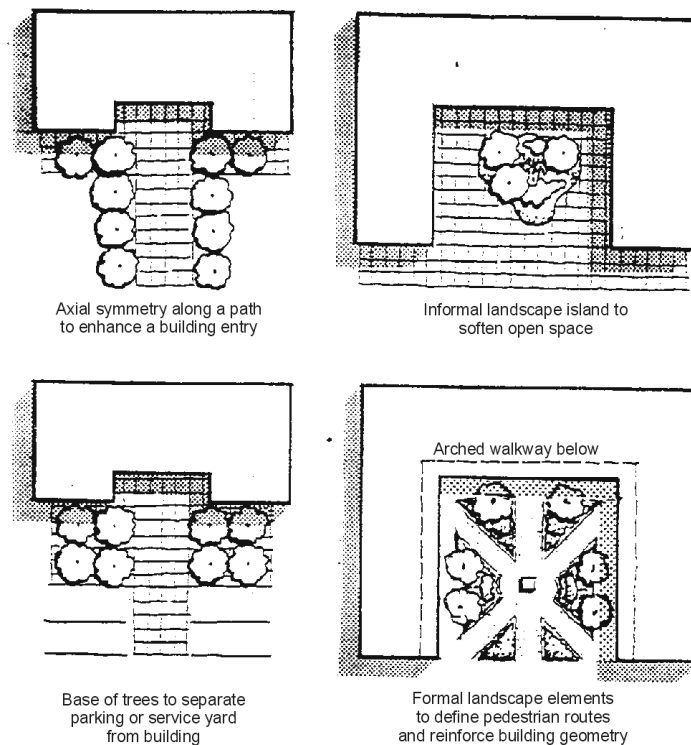
### **Intent:**

- Provide visual relief from large expanses of parking areas and integrate new construction into the natural environment.
- Provide some physical separation between vehicular and pedestrian traffic.
- Provide decorative landscaping as a focal setting for signs, special site elements, and/or pedestrian areas.
- Provide increased areas of permeable surfaces to allow for infiltration of surface water into groundwater resources, reduce the quantity of stormwater discharge, and improve the quality of stormwater discharge.

### **Requirements:**

1. Submit a landscape design plan and be prepared to demonstrate that the plan addresses the following considerations:
  - A unified pedestrian circulation system with amenities and plantings.
  - A coordinated system of open spaces and/or planted areas that provide the required pedestrian areas. The plan should indicate how the various spaces and plantings relate to the project's site design objectives of continuity, variety, activity, etc. The applicant should demonstrate that the landscaping treatment has a "concept" such as the example in Figure 28.
  - Screening of service or unsightly areas.
  - Plantings and/or site features that enhance the building's architectural qualities.
2. In addition, the design should consider the following landscape design objectives where appropriate:

- Where feasible, coordinate the selection of plant material to provide a succession of blooms, seasonal color, and a variety of textures.
- Provide a transition in landscaping design between adjacent sites, within a site, and from native vegetation areas in order to achieve greater continuity.
- Design landscaping to create definition between public and private spaces.
- Design landscaping to provide a transition between built structures (vertical planes) and the site (horizontal planes).
- Use plantings to highlight significant site features and to define the function of the site, including parking, circulation, entries, and open space.



*Figure 12: Examples of landscape designs associated with buildings.*

## B. Street Landscaping

### Guidelines:

1. Sidewalks and pathways should be separated from the roadway by planting strips with street trees wherever possible.
2. Planting strips should generally be at least 5 feet in width (see Figure 8, Tjerne Place cross-section). They should include evergreen shrubs no more than 4 feet in height and/or ground cover in accordance with the City of Monroe Landscape Standards (MMC Chapter 18.78), and canopy-type broadleaf trees placed an



3. average of 25 feet on center. EXCEPTIONS: Where space is limited, planting strips less than 5 feet in width may be permitted by the City.
4. Street trees placed in tree grates may be more desirable than planting strips in key pedestrian areas.
5. Use of trees and other plantings with special qualities (e.g., spring flowers and/or good fall color) are strongly encouraged to unify development in the North Kelsey planning area.
6. Also see Building Orientation guidelines in Chapter 3 and Parking Lot Screening below for areas within the planned development area.

## **C. Parking Lot Screening**

### **Guidelines:**

1. Provide a landscaped drainage/stormwater treatment buffer between the sidewalk/street and parking area where possible. Size the buffer as necessary to perform required stormwater treatment function for a minimum of 20 feet is suggested See Figure 8 Otherwise, a 5-foot wide landscaping buffer consistent with Type III landscaping as specified in MMC Chapter 18.78, to provide a see-through buffer between public streets and parking lots is required; EXCEPTION: The landscape buffer must be 10 feet between parking areas and sidewalks along Chain Lake Road.

## **D. Parking Lot Interior**

### **Guidelines:**

1. Type IV landscaping as specified in MMC Chapter 18.78, is required to provide shade and visual relief while maintaining clear site lines within parking areas.

## **E. Pedestrian Area Landscaping within the Planned Development Area**

These guidelines involve all other pedestrian-oriented spaces and open spaces.

### **Guidelines:**

1. A range of landscape materials—trees, evergreen shrubs, ground covers, and seasonal flowers—shall be provided for color and visual interest.
2. Planters or large pots with small shrubs and seasonal flowers may also be used to separate café seating from traffic flow and create protected areas within the plaza for sitting and people watching.

3. Creative use of plant materials, such as climbing vines or trellises, and use of sculpture groupings or similar treatments are also encouraged.
4. All landscaping shall be as approved by the City.
5. Sun angle at noon and wind pattern should be considered in the landscaping design of the plaza to maximize sunlight areas.

## F. Screening High-Impact Uses

### Guidelines:

1. High impact uses such as sand and gravel mining operations or manufacturing should be screened with a landscape berm.

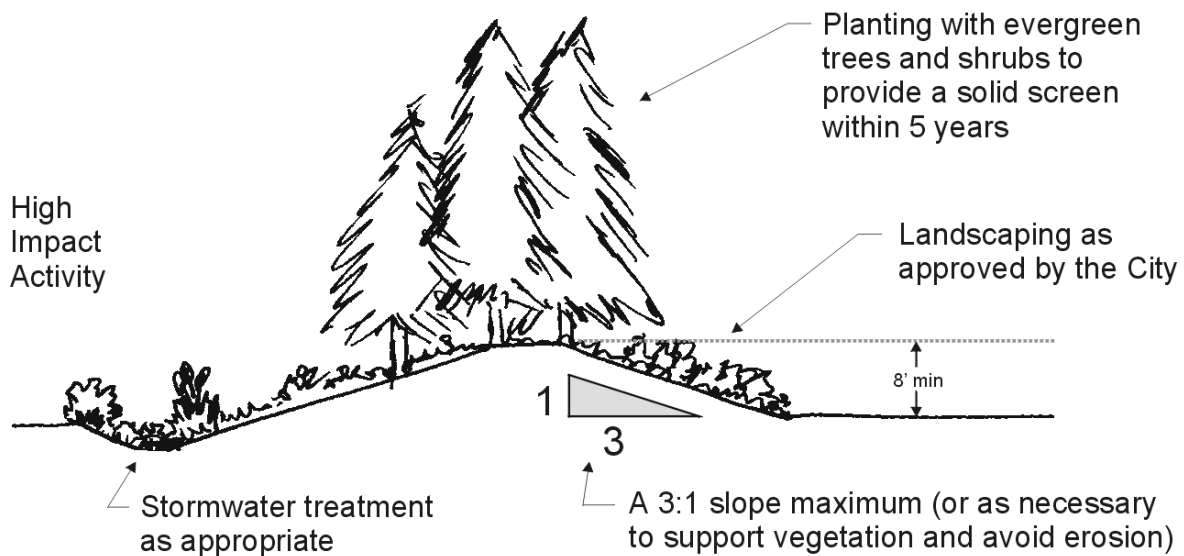


Figure 13: Screening high impact uses.

# CHAPTER 7:

## Signage and Lighting

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### A. Signs

#### Intent:

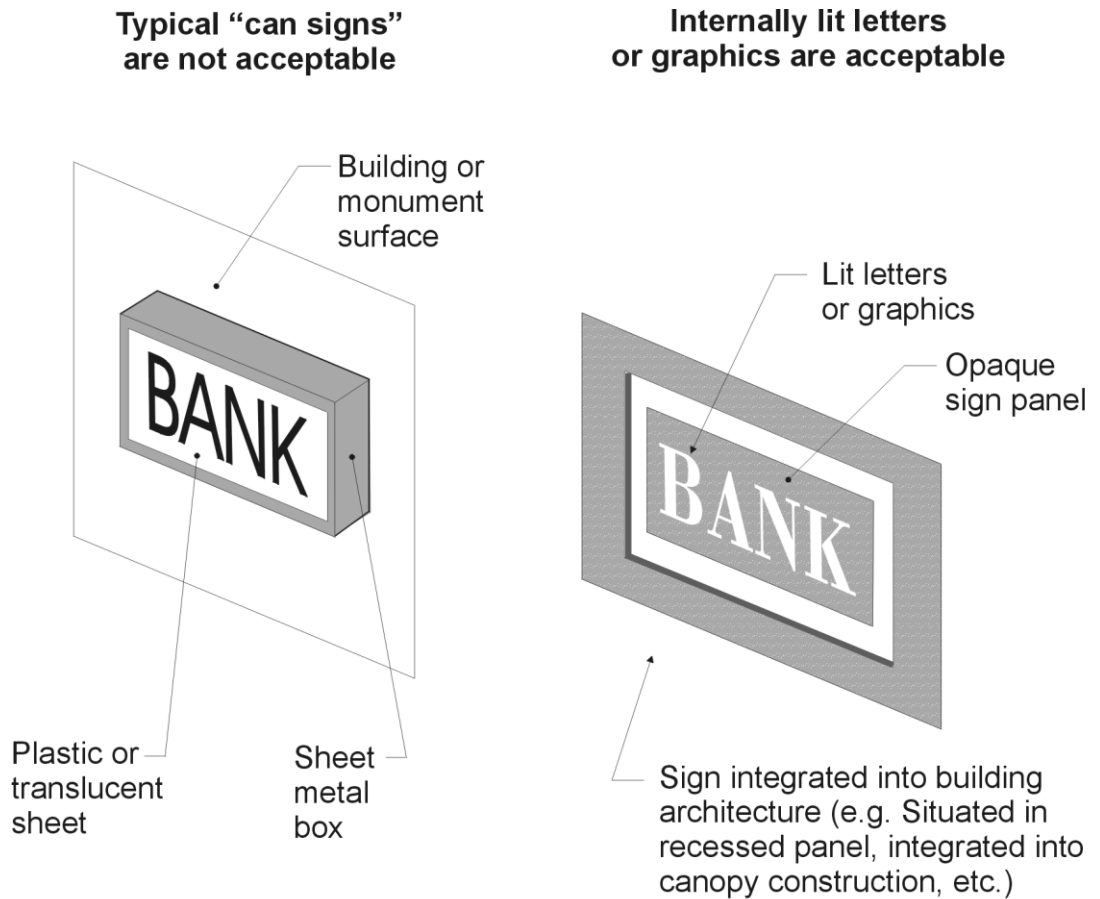
- To encourage signage that is both clear and of appropriate scale for the project.
- To enhance the visual qualities of signage through the use of complementary sizes, shapes, colors, and methods of illumination.
- To provide signage guidelines that meet commercial tenant needs.
- To provide a comprehensive sign program that creates consistent design criteria for the entire North Kelsey planning area.

#### Guidelines:

##### 1. General Standards:

Types of signage fall into three categories: monument site entry signs, tenant signs (large and small), and public and directional signs.

- a. All signage must comply with the following guidelines:
  - Signage must be of high-quality design and materials, consistent with the design of the North Kelsey planning area.
  - Signs may be fabricated of mixed media, including metal reverse-illuminated letters, suspended neon letters, illuminate individual letters, or signs etched or cut out of solid material, such as wood or brass, and illuminated from behind.
  - Signage shall be consistent throughout the planned development area and always complement a building's character (e.g., walls signs should avoid covering building columns).
- b. Non-illuminated mixed-media, and other special mixed-media signs will be subject to approval by the City.
- c. Back-lit translucent awnings and/or awning signs are not permitted.
- d. Back-lit signs with letters or graphics on a plastic sheet (can signs) are not permitted. Generally, these signs are of low quality, easily broken, and not integrated into the building's architecture. However, back-lit letters or graphics that are part of a sign that is integrated into the building's architecture are permitted (see examples on the following pages).



*Figure 30: Appropriate and inappropriate signage.*

- e. The following signs and sign elements are prohibited:
- Pole-mounted signs.
  - Signs employing moving or flashing lights.
  - Signs employing exposed electrical conduits.
  - Visible ballast boxes or other equipment.
  - Signs with luminous plastic letters.
  - Audible or odor-producing signs.
  - Cardboard signs.
  - Roof-mounted and temporary-type signage, such as banners. (Note: Temporary signage for special occasions will be permitted, subject to City approval of both design and duration of display.)
  - Off-premises signs, except public directional and site identification signs.
- f. Cinemas and community centers may have changeable letter signage.

## 2. Public and Directional Signs:

a. *Placement:*

- (1) Public and directional signs may include directional signage and street name markers, pedestrian trail markers, project tenant directories, kiosks, theme elements, and miscellaneous exterior site signage. The design of these elements typically shall share consistent design patterns and express character of the North Kelsey planning project.
- (2) Directional signage shall be required to direct traffic to public parking.
- (3) Pedestrian trail/routes shall be identified.
- (4) Public and directional signs of the North Kelsey planned development area shall be coordinated throughout the site. Information and directional signage design must integrate with the various design areas.

b. *Maximum Size and Quantity Limitations:*

- (1) Parking lot signage for the planned development area shall be limited to one sign per entrance.
- (2) The size and areas of public directional signs shall be reviewed and approved by the City based on design quality, attractiveness and thematic, functional, and scale appropriateness.

3. Tenant Signs:

For purposes of these guidelines, ‘Tenants’ shall be considered those with less than 12,000 square feet of leasable area, and “Large Tenants” shall have a leasable area of 12,000 square feet and greater.

a. *Placement:*

- (1) Wall signs for retail shops shall be located in the storefront area above door height and below the canopy (typically 8 feet above the floor). Wall signage located at building focal points is an exception to this guideline.

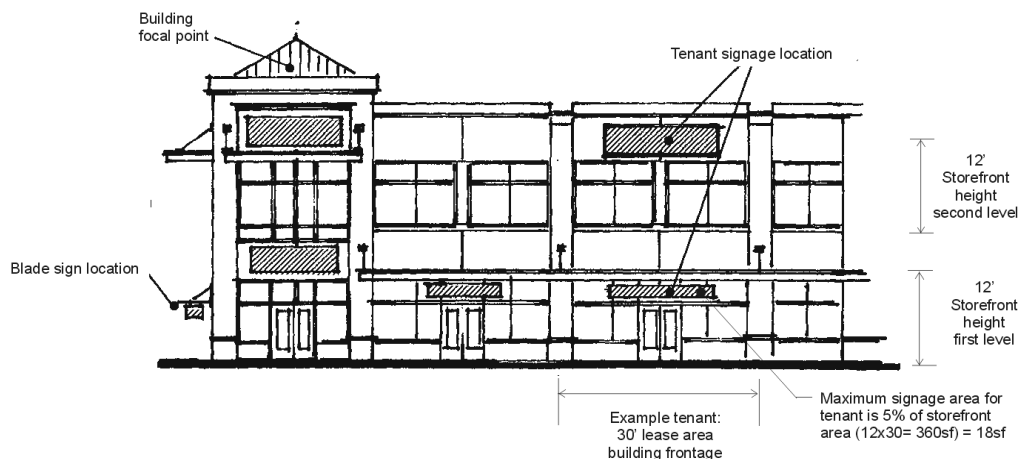


Figure 30: Sign design standards.

- (2) Signs shall be centered between architectural elements and between columns to allow building architecture to be expressed. Signs shall not necessarily be centered on leased premises.
- (3) Signs shall be compatible in scale and proportion with building design and other signs.
- (4) Blade signs, which hang from the canopy, arcade, or building front, may be utilized to increase visibility.
- (5) Upper-level tenants and large tenants may have signs located above the storefronts and below roof-line or cornices, subject to City approval, to accommodate signs visible from adjacent streets.
- (6) Service/secondary signage shall be allowed.
- (7) Signage within the planned development area shall be placed facing public plazas and/or streets. When tenants face two streets or a street and a plaza, signage is allowed on both facades. Each sign is calculated separately and shall conform to all applicable maximum area limitations. Calculated maximum areas are not transferable to other facades. Each tenant is allowed to place signage on no more than two facades.

b. *Size/Design:*

- (1) Office tenant signage maximum is 100 square feet. Approval shall be based on sign design quality, attractiveness, scale appropriateness, and compatibility with the building to which it is attached.
- (2) Letter height shall not exceed 18 inches. Larger first letters up to 24 inches will be permitted.
- (3) Length of the signs shall not be more than two-thirds of the overall leased façade area or less than 3 feet from the demising wall of the leased premises.
- (4) Wall-mounted signs shall not project more than 6 inches from the building.
- (5) Hanging blade signs shall not have an area of more than 3 square feet or exceed 2 inches in thickness. Blade sign area is not included in the signage area. The bottom of the sign should not be lower than 8 feet above ground.
- (6) Service/secondary signage maximum area is 2 square feet.

*Tenant Signage Requirements within the Planned Development Area:*

- Tenant signage calculations apply to tenants with *less* than 12,000 square feet of leasable area.
- Maximum signage area is calculated at 5 percent of storefront area.
- Maximum signage area shall not exceed 24 square feet in total area for any single storefront.
- Each street or plaza frontage is calculated separately.
- Maximum area calculation shall not transfer to other storefronts on other building elevations.

- “Storefront” refers to the first 12 feet above each tenant floor of lease area building frontage facing a street or plaza.

*Example: Storefront area dimensions: 12' x 30' = 360 SF.*

*Signage allowed: 360 SF x 5% = 18 SF*

*Large tenant signage requirements:*

- Tenant signage calculations apply to tenants with *more* than 12,000 square feet of leasable area.
- Maximum signage area is calculated at 5 percent of storefront area.
- Maximum signage area shall not exceed 200 square feet in total area for any single storefront.
- Each street or plaza frontage is calculated separately.
- Maximum area calculation shall not transfer to other storefronts on other building elevations.
- “Storefront” refers to the first 12 feet above each tenant floor of lease area building frontage facing a street or plaza.

*Example: Storefront area dimensions: 12' x 90' x 2 floors = 2,160 SF.*

*Signage allowed: 2,160 SF x 5% = 108 SF*

4. Monument Site Entry Signs for the Planned Development Area:

a. *Placement and Design:*

- (1) Monument Site Entry Signs may be located at major entries connecting to SR-2 and other streets connecting to the planned development area (also see Street Corners/Highly Visible Locations, Chapter 3).
- (2) Signs shall be designed to integrate with adjacent site landscaping.
- (3) Entry signs shall be unified by common design and graphic elements. It is encouraged that monument entry signs share design elements with the public and directional signs throughout the site.
- (4) Entry signs shall identify the North Kelsey planned development. No individual tenant names shall be identified.

b. *Maximum Size and Quantity Limitations:*

- (1) The maximum height and width shall be as approved by the City, with proposals reviewed for scale, design quality, attractiveness, and functional appropriateness for the North Kelsey planning area.

## B. Lighting

### Intent:

- To provide adequate lighting to ensure safety and security.
- To enhance and encourage evening activities.
- To provide a distinctive character to the area.

### Standards:

1. Sidewalks must average between 1 and 2 foot-candles. Primary Pathways must average between 2 and 4 foot-candles. The Focal Plaza, Shopping Corridor, and Pedestrian-Oriented Spaces within the Planned Development Area must be at least 2 foot-candles (also see Chapter 3). Parking areas must average at least 1 foot-candle. Parking lot pathways must be average at least 2 foot-candles.
2. The color of light should be considered in the lighting design. For example, metal halide is recommended for general usage at building exteriors, parking areas, and pedestrian courts as well as in streetlamps and for lighting street trees. Low-pressure sodium, which casts a yellow light, is not recommended except for streets and parking lots.
3. Uplighting on trees and provisions for seasonal lighting are encouraged.
4. All efforts to reduce glare into the North Kelsey planning area and surrounding community from street and parking area lights should be undertaken.
5. Accent lighting on architectural and landscape features is encouraged to add interest and focal points.
6. Pedestrian-scaled lighting is required within the planned development area along all streets and in all public plazas and courts. Pedestrian-scaled lighting fixtures are generally 12-14 feet and of a character complementary to the building architecture.
7. Parking area lighting shall not exceed 25 feet in height and shall be baffled to minimize glare and spillage into the surrounding community.



# Definitions

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*Articulation* – Articulation is design emphasis placed on a particular architectural feature by special details, materials, change in building plane (recessed or extended from building surface), contrast in materials, or decorative artwork.

*Blank Walls* - Walls subject to "blank wall" requirements meet the following criteria:

- Any wall or portion of a wall that has a surface area of 400 SF of vertical surface without a window, door, building modulation as defined below or other architectural feature (see figure below for measuring methods).
- Any ground level wall surface or section of a wall over 4' in height at ground level that is longer than 15' as measured horizontally without having a ground level window or door lying wholly or in part within that 15' section (see below).

*Human Scale* - The term "human scale" refers to the perceived size of a building relative to a human being. A building is considered to have "good human scale" if there is an expression of human activity or use that indicates the building's size. For example, traditionally sized doors, windows, and balconies are elements that respond to the size of the human body, so these elements in a building indicate a building's overall size.

*Modulation* - In the design guidelines, modulation is a stepping back or projecting forward of portions of a building face within specified intervals of building width and depth, as a means of breaking up the apparent bulk of a structure's continuous exterior walls.

*Pathways* – Refer to any pedestrian route other than a *sidewalk*.

*Pedestrian-Oriented* - Can be small to large widening of walking space, landscaped areas, areas for outdoor dining, or small play areas (see guidelines and requirements for *Pedestrian-Oriented Spaces* in Chapter 3).

*Sidewalks* - Refer to concrete pedestrian routes adjacent to public right-of-ways.

*Weather Protection* – Architectural features such as an awning, marquee, or canopy that protects pedestrians from rain and sunlight.

## MEMORANDUM

To: Community Development Department

From: Bob Bengford AICP, Partner

Date: September 15, 2020

**Re: MAKERS summary review of North Kelsey Development Plan  
Design Guidelines Update**

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**Commented [BB1]:** This September update just includes new sections on Chapters 6 and 7 (no changes to earlier sections/recommendations)

MAKERS was asked to assist the City in evaluating the existing design guidelines and proposed edits to the North Kelsey Design Guidelines based on the evolving context and goals for the project area and best practice for design standards and guidelines in the region. This memo includes detailed comments and suggestions of each section.

Discussion comments are written in *italics* throughout and we've provided comments in the column on numerous items. For the most part, suggested new text is tracked.

A review of Chapters 6 and 7 aren't included yet – the only comment thus far (from City) involves signs – to only comment on design elements. We will follow up with a Part II of our analysis to include those chapters as we have available time (and as required by the City's timeline.

## Chapter 1: Introduction

### A. Purpose

*No comments or suggested changes.*

### B. Intent

*Staff's suggested edits are tracked in black. Our suggested adjustments are tracked in red. Italics*

- ~~Fosters a vibrant~~~~Provides a visible~~ and accessible ~~mixed-use residential and~~ district for the City of Monroe. The term “vibrant” seems more useful than simply “visible”.
- Enhances ~~downtown~~ circulation for pedestrians and vehicles.
- ~~Connects and integrates other downtown activities.~~ While I can understand the reason for including this statement initially, it’s not entirely clear what this means. Consequently, it doesn’t add much value now.
- Accommodates retail, residential, office, and service development of various sizes and character as is appropriate for Monroe’s small town character with design quality of the highest caliber.
- Enhances the town’s identity as a regional attraction.

### C. Application of the Design Guidelines

*The three paragraphs here combined with Figure 1 are confusing. We suggest adding a hatched or other clear pattern to delineate the “Planned Development Area” to clarify the boundaries of such area and better synch with suggested text edits below:*

The design guidelines apply to all new construction in the North Kelsey Planning Area (Figure 1). Exceptions:

- ~~The sole exception to the guidelines involves if~~ Interior remodels are exempt.
- All properties that are outside the planned development area (hatched area in Figure 1), but within the North Kelsey Planning Area are only subject to Chapters 5 through 7 of these Design Guidelines. ~~This includes the privately owned parcels within the outlined area. Last sentence isn’t necessary.~~

The guidelines are intended to supplement other requirements in the Monroe Municipal Code. Where the guidelines and zoning ordinance standards conflict, the City shall determine which regulation applies.

~~All properties that are outside the development area, but within the North Kelsey Planning Area are subject to Chapters 5 through 7 of these Design Guidelines. This includes the privately owned parcels within the outlined area. Text moved up to second bullet.~~

~~The specific planned development area is comprised of properties as reflected in Figure 1, Applicable Properties. This is duplicative.~~

#### D. Interpreting the Design Guidelines

MAKERS has adjusted our recommended approach to crafting guidelines since the adoption of the original document. We advocate for provisions that provide the right combination of both predictability and flexibility by:

- Including clear minimum “standards”
- Offering multiple ways to meet the standards (in some, but not all cases)
- Offering strategic departures – or alternative ways of meeting the certain (but not all) standards provided such designs meet the intent of the standard and any special departure criteria that may be provided.

By integrating provisions of flexibility with these standards, we can still collectively refer to the provisions in the document “guidelines”. However, to reduce confusion, we tend to now simply use the phrase “design standards” over “design guidelines” when crafting new provisions, except for provisions that are reviewed by a specialized “design review board” composed of appointed citizens that have some expertise in the design and development fields.

The current guidelines allow applicants to depart or vary from ANY provision. This allows a lot of flexibility for the applicant, but creates the potential to water down the provisions and could facilitate numerous negotiation challenges for the City (and greater potential for inconsistent application of the guidelines). By making the departures “strategic”, this arrangement offers greater predictability to all involved.

On the other hand, if the City feels that this current and more flexible approach is working well, then we’d suggest keeping the language as is – and simply focus on the details of the guidelines and perhaps add special criteria for approving alternative approaches to the most critical guidelines.

Below is alternative text for this section emphasizing our suggested new approach:

The guidelines herein include the following elements:

- A. Intent statements, which are overarching objectives.
- B. Required provisions use words such as “must” and “is/are required”.
- C. Voluntary provisions use words such as “should” or “is/are recommended,” signifying voluntary measures.
- D. Departures are provided for specific provisions. They allow alternative designs provided the City determines the design meets the intent of the guidelines and other applicable criteria.
- E. This division contains some specific guidelines that are easily quantifiable, while others provide a level of discretion in how they are complied with. In the latter case, the applicant must demonstrate to the City, in writing, how the project meets the intent of the standard(s).

#### E. Review Process

No new suggestions.

## Chapter 2: Site Configuration

Since the street network has been completed here, much of the content in this chapter is no longer necessary. Here's some suggested changes for consideration (replace current content with the following):

The development must address the following principles.

1. Locate and design buildings to create a distinctive pedestrian-friendly environment along Tjerne Place (add cross-reference). This is critical to the long term success of this area – particularly in setting the tone for what could happen in the future on the south side of the road.
2. Utilize the curves in Tjerne Place to add a sense of drama to the development via building siting, massing, and detailing plus special streetscape and landscaping design. Figure 2 below illustrates examples of how this can be accomplished. This is a unique opportunity given the shape and delineation of Tjerne Place – and is not addressed in the existing guidelines.
3. Locate and design parking to maximize opportunities for shared parking. This statement simplifies the existing language.

The project proponent must demonstrate that the overall site layout and ~~circulation system design~~ accomplishes these ~~goals-principles~~ to the City's satisfaction. This statement still works well. It just needs some consistency updates.



Figure 2. Example site plan (left image) emphasizing building locations to create a distinctive pedestrian-friendly environment along Tjerne Place. Note the curve of the buildings colored gold to hug Tjerne Place, helping to create a sense of drama. Also, the turrets illustrated in the sketch on the right add drama to the site and provide signage opportunities for tenants.

## Chapter 3: Site Planning

### A. ~~Public~~ Open Space

*Since the uses here now include residential development, we'd suggest that this section address both public and private open space. Otherwise, the current language in this section doesn't add all that much of value. We'd suggest that commercial development include a base minimum amount of pedestrian-oriented space and include some design standards for the design of that space. The text below is a suggested complete update of the existing provisions.*

#### Intent:

- To provide pedestrian-oriented spaces in commercial areas that enhance the employees' and public's opportunity for active and passive activities, such as dining, resting, people watching, and recreational activities.
- To enhance the development character and attractiveness of commercial development.
- To create open space that contributes to the residential setting.
- To create useable open spaces for residents' leisure, play, exercise, and socializing.

#### Guidelines:

1. Pedestrian-oriented space for non-residential development. New developments with non-residential must provide pedestrian-oriented space (see subsection A.2 below) equal to at least five-percent of the non-residential floor area. Portions of sidewalks that are wider than 12-feet and which meet the guidelines of pedestrian-oriented open space may be counted toward this requirement.

DEPARTURE: Open space area may be reduced to 2-1/2-percent of the non-residential floor area if the City finds the project includes exceptional design features and elements that meet the intent of the guidelines. This includes open spaces that feature a combination of design (site materials, amenities, and configuration) and location/context that clearly exceed typical plaza designs found in the region.

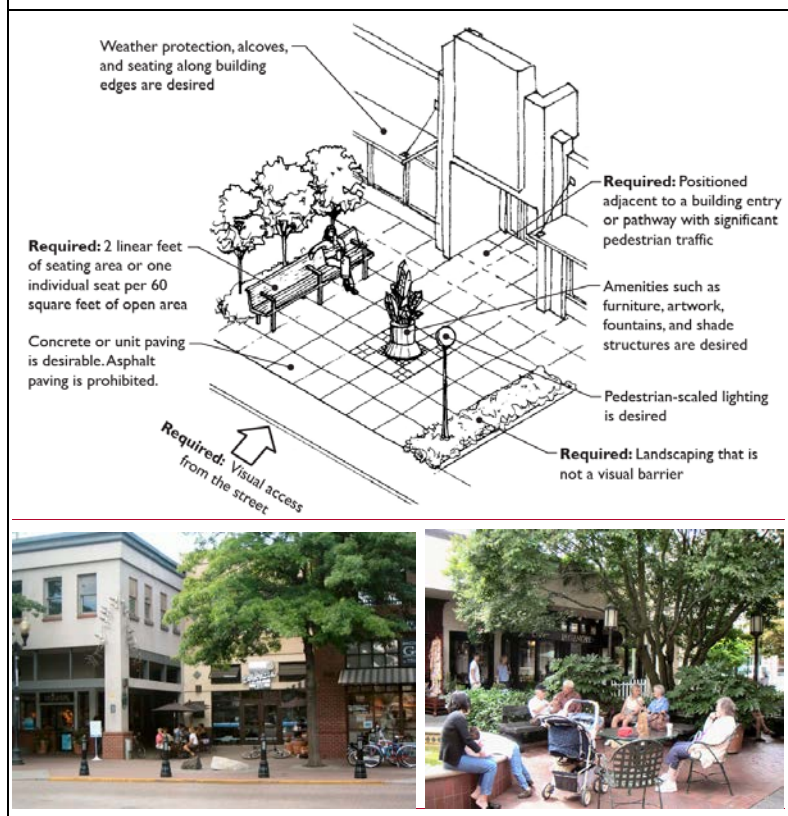
Figure 3. Example of site development integrating the required pedestrian-oriented space.

TO ADD SITE PLAN ILLUSTRATING A BUILDING MEETING THE APPLICABLE 5% PED SPACE REQUIREMENT.

2. Pedestrian-oriented open space design criteria. This subsection describes the requirements and desired characteristics of pedestrian oriented open space.
  - a. Required pedestrian-oriented open space features.
    - i. Visual and pedestrian access into the site from a street, private access road, or non-vehicular courtyard.

- ii. Paved walking surfaces of either concrete or approved unit paving. Form-in-place pervious concrete paving is allowed.
  - iii. Lighting must conform to MMC Chapter 15.15.
  - iv. The spaces must be located in or adjacent to areas with significant pedestrian traffic to provide interest and security, such as adjacent to or visible from a building entry.
  - v. At least two-feet of seating area (a bench or ledge at least 16-inches deep and appropriate seating height) or one individual seat per 60-square-feet of plaza area or open space.
  - vi. Landscaping components that add visual interest and do not act as a visual barrier. This could include planting beds, raised planters, and/or potted plants, or both.
- b. Desirable pedestrian-oriented open space features:
- i. Pedestrian amenities, such as site furniture, artwork, drinking fountains, shade structures kiosks, or other similar features.
  - ii. Adjacent buildings with transparent windows and doors covering at least 50-percent of the façade between 30-inches and ten-feet above the ground level.
  - iii. Pedestrian weather protection, alcoves, seating, or other features along building edges to allow for outdoor gathering.
  - iv. Pedestrian-oriented open spaces may include LID BMPs, like rain gardens, in up to 25-percent of the required open space.
- c. Features prohibited within a pedestrian-oriented open space:
- i. Asphalt pavement.
  - ii. Adjacent service areas (e.g., trash areas, loading docks) that are not separated with landscaping, as required by \_\_\_\_\_.
  - iii. Adjacent chain-link fences.
  - iv. Adjacent "blank walls" without "blank wall treatment" (see \_\_\_\_\_).
  - v. Outdoor storage.

**Figure 4. Example pedestrian-oriented space.**



**3. Multifamily open space. Developments including multifamily units must provide on-site recreation space that meets the provisions of MMC 22.42.070(G) plus the following supplemental design provisions.**

**Common open space. At least 50-percent of the required on-site recreation space must meet the following common open space provisions. Common open space refers to open spaces that are accessible to all tenants of a development, but may not be accessible to the general public. Common open spaces can include landscaped courtyards or upper level/rooftop decks, entrance plazas, gardens with walkways, children's play areas, pools, and water features. Accessible areas with native vegetation and areas used for storm water**



retention, infiltration, or other multipurpose recreational and/or green spaces that meet the design criteria herein may qualify as common open space.

- a. Common open space must be located in centralized areas that are visible from tenants within the development (rooftop decks meeting the provisions below are an exception).
- b. Required setback areas must not count as common open space unless the design of the space meets the guidelines herein.
- c. Common open space must feature paths or walkable lawns, landscaping, seating, lighting, and play structures, sports courts, or other pedestrian amenities to make the area more functional and enjoyable for a range of users.
- d. Common open space must be separated from ground level windows, streets, service areas and parking lots with landscaping, fencing, and/or other acceptable treatments that enhance safety and privacy for both the shared open space and dwelling units.
- e. When possible, the space should be oriented to receive sunlight, facing east, west or preferably south. Provisions for shade, however, must also be integrated in spaces that will be exposed to extensive sunlight.
- f. Stairways and service elements located within or on the edge of shared open space must not be included in the open space calculations.
- g. Shared porches may qualify as shared open space, provided they are at least eight-feet in depth and 96-square-feet in total area.
- h. The space must be accessible to all residents of the development.
- i. LID BMPs, like rain gardens, may be integrated into the design of the space and may occupy up to 25-percent of the required common open space.

**Figure 5. Example common open space.**

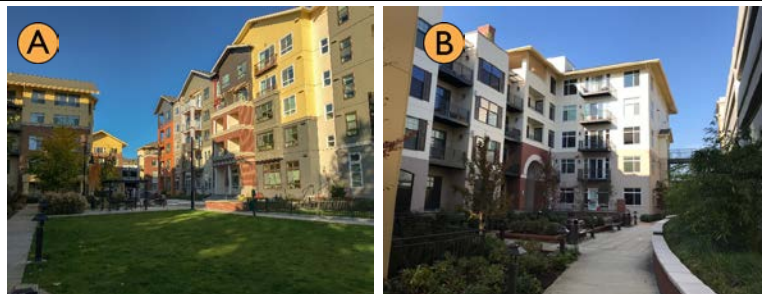


Image A includes a common green area and separate fenced off-leash dog area (background to the right). Image B is a courtyard with includes pathways, seating areas, landscaped beds, and semi-private spaces for adjacent ground level units.

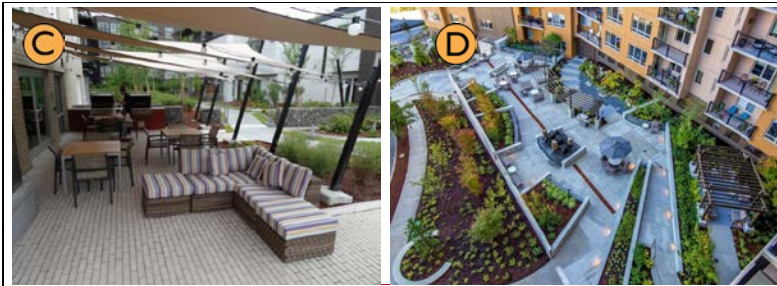


Image C includes a covered gathering space with outdoor grills adjacent to a landscaped commons with a central pathway. Image D includes a landscaped plaza with multiple seating areas and an outdoor fireplace.

## B. Building Location & Orientation


**Intent:** No comments or suggested changes.

**Guidelines:** Suggest a complete update of this section to enhance and clarify provisions:

The building location and orientation guidelines herein are intended to create a pedestrian-friendly environment that encourages the development of active storefronts directly adjacent to the sidewalk, but allows some flexibility to the setback and design of facades. Generally, the closer a façade is to a sidewalk, the more stringent the design provisions. For example, facades directly adjacent to a sidewalk must meet strict storefront design guidelines. A façade setback 15-feet from a sidewalk, however, will have reduced window transparency requirements, provided landscaping and other design features are included to mitigate the impact of any blank walls and enhance the streetscape. A similar approach applies to other visible facades; the closer a walkway is to an internal pathway or other paved area, the more stringent the standards will be for the façade design (most notably transparency). Departure provisions are included to allow additional design flexibility provided the applicant can successfully demonstrate that alternative design treatments meet the intent of the guidelines and any additional design criteria.

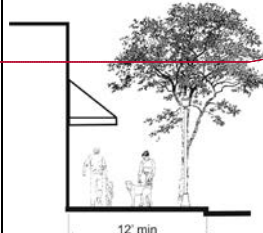
I. Storefront design. Buildings may be located directly adjacent to a sidewalk or internal pathway provided they meet the storefront guidelines in Table B-I below.

**Commented [BB2]:** Emphasizing its “optional” not required!

<b>Table B-I. Storefront Design.</b>		
The ☺ symbol refers to DEPARTURE opportunities. See special departure criteria in the right column.		
<b>Element</b>	<b>Standards</b>	<b>Examples &amp; Departure Criteria</b>
<b>Ground level</b>		
Land use	Non-residential uses specified in MMC Table 22.26.040 are required except for lobbies and common areas associated with residential uses on upper floors.	
Floor to ceiling height	13' minimum to ensure that the space is viable and attractive for a wide range of non-residential users	
Non-residential space depth	40' minimum average for uses facing Tjerne Place ☺ and 20' elsewhere. Again, the purpose is to ensure that such spaces are viable and flexible to a wide range of non-residential users.	
<b>Building entrances</b>	Primary building entrances must face the street or internal pathway. For corner buildings, primary entrances may face the street corner.	

**Commented [BB3]:** I've seen this go up to 15' but 13' seems reasonable here for a minimum.

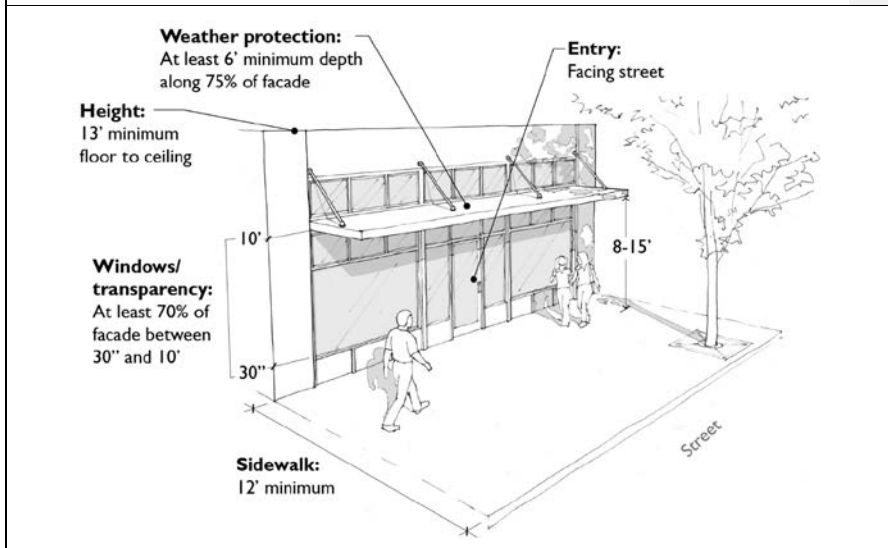
**Commented [BB4]:** Again – trying to create viable and flexible space. The “average” dimension is important – as portions of the space may need to be smaller to fit other building/program elements in. We've added a departure option that allows some negotiation – and the city's ability to say no if they feel proposal doesn't meet the purpose.

<b>Table B-1.</b> <b>Storefront Design.</b>		
The ➡ symbol refers to DEPARTURE opportunities. See special departure criteria in the right column.		
Element	Standards	Examples & Departure Criteria
<b>Façade transparency</b>	At least 70% of the transparency zone (between 30"-10' above sidewalk/pathway grade). ➡ Ground-level window area for storefronts and other non-residential uses that is covered, frosted, or perforated in any manner that obscures visibility into the building must not count as transparent window area. Also, mirrored glass and highly-reflective or darkly-tinted windows must not be counted as transparent windows.	Departure criteria: Alternative designs must provide strong visual interest to the pedestrian and mitigate the impacts of any blank walls. No less than 40% transparency will be considered.
<b>Weather protection</b>	Weather protection over the sidewalk/pathway is required along at least 75% of the storefront façade, and it must be a minimum of 6' deep and have 8' to 15' of vertical clearance. ➡ Weather protection must not interfere with street trees, street lights, street signs, or extend beyond the edge of the sidewalk.	Departure criteria: Alternative design treatments must provide equivalent weather protection benefits.
<b>Sidewalk/pathway width</b>	12' minimum between the curb edge and the storefront façade (including clear/buffer zone with street trees). Setbacks and utility easements must also be considered and may result in a larger minimum sidewalk width.	

**Commented [BB5]:** We've also used a minimum "average" of 5 or 6 feet – which allows some flexibility with design.

**Commented [BB6]:** If sidewalk standards for street/frontage are less, they will just need to set building back a little to help comply. Upper floors could cantilever to ROW or other minimum walkway width. We can add a provision on that if needed to clarify (cantilevering).

**Figure**  
**Storefront vision and key standards.**



**2. Other non-residential building frontages.**

**Table B-2.**  
**Design of Other Non-Residential Building Frontages.**

The ☞ symbol refers to DEPARTURE opportunities. See special criteria set forth in the right column.

Element	Standards	Examples & Departure Criteria
<b>Ground level</b>		
<u>Land use</u>	See the uses specified in in MMC Table 22.26.040.	
<b>Building entrances</b>	Buildings facing Tjerne Place must feature a public building entry visible from Tjerne Place.	
<b>Façade transparency</b>	Buildings within 10' of a sidewalk on Tjerne Place or within 5' of an internal pathway must feature at least 40% transparency within the transparency zone. ☞	The transparency zone is between 30" and 8' above sidewalk/internal pathway grade.

**Commented [BB7]:** I'm sure the tire store would go for this approach.

**Commented [BB8]:** Emphasis on "visible". Doesn't have to be on that Tjerne Place façade.

**Commented [BB9]:** Provisions here allow some flexibility for tire store depending on setback from sidewalk. I've seen plenty of Les Schwabs that would meet this.

<b>Table B-2.</b> <b>Design of Other Non-Residential Building Frontages.</b>		
The ☞ symbol refers to DEPARTURE opportunities. See special criteria set forth in the right column.		
Element	Standards	Examples & Departure Criteria
	Buildings within 20' of sidewalk or 10' of an internal pathway must feature at least 25% transparency within the transparency zone. ☞ Ground-level window area for storefronts and other non-residential uses that is covered, frosted, or perforated in any manner that obscures visibility into the building must not count as transparent window area. Also, mirrored glass and highly-reflective or darkly-tinted windows must not be counted as transparent windows.	Departure criteria: Alternative designs must provide strong visual interest to the pedestrian and mitigate the impacts of any blank walls. No less than 50% reduction in required transparency will be considered.
<b>Weather protection</b>	Weather protection at least 5' deep is required over the full width of all public building entries. Such weather protection must have 8' to 15' of vertical clearance.	Departure criteria: Alternative design treatments must provide equivalent weather protection benefits.

Commented [BB7]: I'm sure the tire store would go for this approach.

3. Residential building frontages, where permitted.

<b>Table B-3.</b> <b>Design of Residential Building Frontages.</b>	
The ☞ symbol refers to DEPARTURE opportunities. See special criteria set forth in the Standards column as noted.	
Element	Standards
<b>Direct access</b>	For buildings within 10-feet of a Tjerne Place sidewalk or within 5-feet of an internal pathway, ground level dwelling units must all have individual ground-related entries accessible to those elements.
<b>Building/dwelling unit setback and elevation</b>	Provide privacy for people living in the adjacent dwelling units through all of the following measures: <ul style="list-style-type: none"> <li>• Minimum setback: 10' ☞ (measured from the edge of the walkway or common open space to the building). Departures will be considered for setbacks to be reduced as low as 5' provided the design meets the provisions below, enhances residents' privacy and enhances the design of the streetscape for pedestrians.</li> <li>• Where the façade is within 10' of a sidewalk or internal pathway, all the following design features must be integrated to help to improve privacy and enhance their relationship to the public or semi-public realm:                             <ul style="list-style-type: none"> <li>○ The ground level of adjacent residential units must be raised above the sidewalk grade by an average of 30-inches.</li> </ul> </li> </ul>

<p><b>Table B-3.</b>  <b>Design of Residential Building Frontages.</b></p>	
<p>The ➡ symbol refers to DEPARTURE opportunities. See special criteria set forth in the Standards column as noted.</p>	
Element	Standards
	<ul style="list-style-type: none"> <li>○ Provide a physical "threshold" feature such as a hedge, retaining wall, rockery, stair, gate, railing, or a combination of such elements on private property that defines and bridges the boundary between public right of way and the dwelling unit.</li> <li>○ Landscaping planters must be integrated into the setback area between the dwelling unit and the adjacent sidewalk or internal pathway.</li> </ul>

4. Building and parking lot locations along Tjerne Place. Parking lots are limited to a maximum of 50-percent of Tjerne Place street frontage (north side only). Internal access streets do not qualify as a parking lot for the purposes of this guideline. Figure 6 below illustrates an example development configuration that meets this provision (and what qualifies as a parking lot). DEPARTURES will be considered provided buildings occupy no less than 35-percent of the street frontage and there must be an acceptable tradeoff in terms of the amount and quality of building frontage and enhanced design of parking lot buffer treatment (design must go well beyond minimum landscape buffer requirements, for example)

**Commented [BB10]:** Should be workable for tire store – just can't have it set back off of street with parking and service bays in front.

**Figure 6. Example building configuration that meets Tjerne Place building frontage provisions.**



NOTE: TO ADD CLARIFYING NOTES REGARDING WHAT COUNTS AS A PARKING LOT AND WHAT DOESN'T. I'M THINKING THE THREE ACCESS DRIVES OFF TJERNE PLACE WON'T COUNT – THOUGH I COULD BE CONVINCED OTHERWISE!

### C. Parking Areas

**Intent:** Suggested changes are consistent with Staff's edits:

- ~~To provide convenient parking areas that encourage people to leave their cars and walk throughout the North Kelsey Planning Area.~~
- ~~To provide more flexibility in the design of the development by relaxing existing City parking standards.~~
- To provide parking areas that do not diminish pedestrian and visual qualities of the site.
- To maintain the built street edge through effective screening of all parking lots.
- ~~To minimize the impacts of driveways.~~
- To provide safe pedestrian access through parking lots.
- To provide landscaping elements within and around parking lots to mitigate the visual impacts of parking lots, provide shade and environmental benefits, and enhance the character of development.

**Intent:** Suggested changes are consistent with Staff's edits (no other changes other than what's shown):

- ~~2. Parking requirements for retail uses shall be relaxed to 1 space per 250 square feet of gross floor area. The City may consider special provisions for joint use of parking when two activities are less likely to occur simultaneously (e.g. office uses and entertainment facilities).~~
4. Pathways through parking lots should be provided. Pathways and crosswalks should be provided along every fourth parking isle or at intervals of less than 150 feet. Pathways through parking areas should be separated from vehicle parking and travel lanes by use of contrasting surface materials, which may be raised above the level of the vehicular surface. Parking area pathways should be at least 4-5 feet in width.

### E. ~~Street Corners/Highly Visible Locations~~

*Given what's been developed already and suggested changes above, we agree with staff that this section probably is no longer necessary. Delete entire section E.*



## Chapter 4: Circulation

### A. Sidewalks and Pathways

Suggested changes and comments are below.

3. Acceptable sidewalk widths may range from 4-5 to 12 feet depending on adjacent uses and anticipated pedestrian activity. Sidewalks along major connector routes such as North Kelsey Street or Chain Lake Road should be at least 8 feet in width to accommodate two couples passing each other.
6. Pathways that provide key access to other key sites are termed "Primary Pathways." Primary pathway surfaces should be at least 15 feet in width to accommodate fire apparatus access and groups of people.

Also, Figure 7 might be clarified whether it's a requirement or "example" as shown?



Figure 7. Example pedestrian network for the North Kelsey Planned Development Area.

**Commented [BB11]:** It appears that these sidewalks are already built?

**Commented [BB12]:** Is there a particular reason that we chose 15-feet here? Even the secondary pathway width seems....ambitious. Maybe it's a carryover to the original North Kelsey concept?

### B. Bicycle Circulation and Amenities

We agree with staff's suggestion to delete the following two items below, though adjustments and clarifications are warranted.

- I. Safe bicycle access should be provided within each public right-of-way developed within the North Kelsey planning area. The City will consider the following options:
  - a. ~~Bike Lanes. Standard bike lanes are 5 feet in width. This is the preferred option for Chain Lake Road (where there is sufficient right-of-way width) since bicycle lanes would connect with planned bicycle lanes north of the site.~~
  - b. ~~Wide Curb Lanes. This involves 14 foot travel lanes rather than the standard 11 or 12 foot lanes so cyclists can safely share the road with vehicles. Although such wide curb lanes are often striped, they are not signed or officially designated as bike lanes. With limited space, this is often the most effective way to provide safe bicycle access.~~
  - c. Multi-Use Pathway. This combines bicycle and pedestrian access on an asphalt pathway separated from the roadway. Ideally, such a multi-purpose pathway should be 12 to 14 feet in width. Where space and use are expected to be limited, an 8-foot wide pathway (with center striping) may be acceptable. Pathway design should ensure adequate site distance.

**Commented [BB13]:** Again – it appears all the major roads here are completed. But with retaining "c" below is the thought that the sidewalk on the north side of Tjerne Place could be upgrade to this 12-14' pathway?

Either way – we'll need to adjust the language here.

## Chapter 5: Architectural/Building Design

### A. Architectural Concept

Guidelines: Suggested changes are consistent with Staff's edits:

- I. The buildings proposed for the North Kelsey planned development area should be based on a comprehensive architectural concept that achieves the intent statements above. Specifically, the design of the specific buildings should address:

- Pedestrian interest and comfort along the perimeter of open spaces and pedestrian connections.
- For large buildings (over 3-stories or more than 20,000 square feet), integrating modulated elements that create a composition, reduce the perceived scale of the building, and add visual interest.
- Articulating visible facades of all buildings to reduce the perceived scale of buildings, integrate human-scaled design elements, and add depth and richness to facades. Building massing and elements relative a human body.
- The perceived massing of the building relative to nearby structures, open spaces, and landscape elements.
- Monroe's architectural and cultural setting.
- The variety of sequential experiences and design characters within the site.

While the individual design guidelines in this section address some of these issues specifically, the intent of this guideline is to encourage the designers to consider how the various aspects of the design work together. Applicants should be prepared to demonstrate how the proposed buildings respond to the intent statements. The City will review applicants' proposals and determine whether or not they meet the intent.

### B. ~~Human/Pedestrian Scale~~Building Massing & Articulation

*As implied by staff, we suggest a complete revision of this section to address key massing issues while allowing options. Language to consider:*

#### Intent.

- To employ façade articulation techniques that reduce the perceived scale of large buildings and add visual interest from all observable scales.
- To create clear and welcoming building entries.

#### Guidelines.

- I. All buildings must include façade articulation features at designated maximum intervals to create a human-scaled pattern. This guideline applies to all building elevations facing streets, internal pathways, and parking lots.

#### a. The maximum horizontal length of intervals:

- i. Residential, multi-tenant retail and shell commercial buildings: 30 feet.
- ii. Single-tenant commercial buildings and office buildings: 50 feet.

**Commented [BB14]:** The below provisions are clear, predictable, and battle tested. Very important. The departure provision allows some flexibility – but city has power to say no.

**Commented [BB15]:** Consider tire store context implications re location/design. Where façade where service bays face will have a harder time meeting this - so we'll want to think about it. May be good to look at some other tire store examples on google earth and provide suggested layouts.

- b. At least three of the following articulation features must be employed in compliance with maximum interval guidelines above:
  - i. Use of windows and/or entries.
  - ii. Use of weather protection features.
  - iii. Use of vertical piers/columns (applies to all floors of the façade, any excluding upper level stepbacks).
  - iv. Change in roofline per subsection B.4 below.
  - v. Change in building material, siding style, and/or window pattern (applies to all floors of the façade, excluding any upper level stepbacks).
  - vi. Vertical elements such as a trellis with plants, green wall, art element that meet the intent of the standard.
  - vii. Providing vertical building modulation of at least 12-inches in depth if tied to a change in roofline per subsection B.4 below or a change in building material, siding style, or color. Balconies may be used to qualify for this option if they are recessed or projected from the façade by at least 18-inches. Juliet balconies or other balconies that appear to be tacked on to the façade will not qualify for this option unless they employ high quality materials and effectively meet the intent of the guidelines.
  - viii. Other design techniques that effectively reinforce a pattern of small facades compatible with the building's surrounding context.

DEPARTURES will be considered on the number and/or type of articulation treatment provided they meet the intent of the guidelines and the design criteria below in subsection B.3. For example, a departure may propose a design with only two articulation features instead of three or the articulation features exceed the maximum articulation interval.

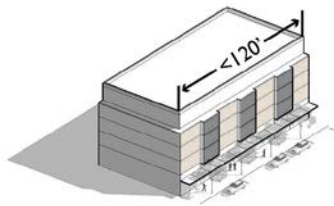
- 2. Maximum façade width. Buildings containing one or more visible building elevations wider than 120-feet must include at least one of the following features to break up the massing of the building and add visual interest. This guideline applies to all building elevations facing streets, internal pathways, and parking lots.
  - a. Provide vertical building modulation at least six-feet deep and 15-feet wide. For multi-story buildings, the modulation must extend through at least one-half of the building floors.
  - b. Use of a contrasting vertical modulated design component featuring all of the following:
    - i. Utilizes a change in building materials that effectively contrast from the rest of the façade.
    - ii. Component is modulated vertically from the rest of the façade by an average of six-inches.
  - c. Façade employs building walls with contrasting articulation that make it appear like multiple distinct buildings. To qualify for this option, these contrasting façades must employ all of the following:

**Commented [BB16]:** Very important tool – just look at the bad examples illustrated below to see what could happen without such tools.

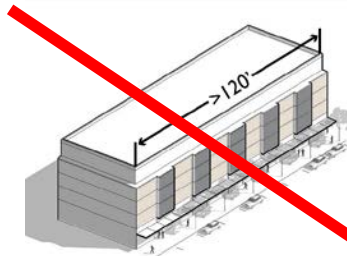
- i. Different building materials and/or configuration of building materials.
- ii. Contrasting window design (sizes or configurations).
- d. Elevations with prominent bend or curve at least 25-degrees, so that no one straight segment exceeded 120-feet.

**Figure 5.B.2**

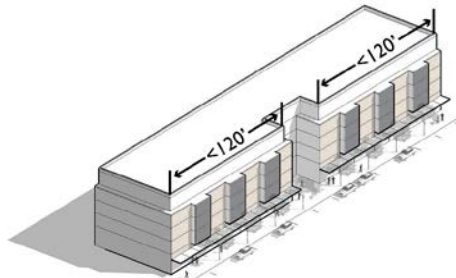
**Illustrating maximum façade width guidelines and good and bad examples.**



Less than 120' wide: Meets guideline.



More than 120' wide: Does not meet guideline.



Building incorporates a courtyard along the façade (technique #1 noted above) to effectively break it up into smaller components: Meets guideline.

**Figure 5.B.2**

**Illustrating maximum façade width guidelines and good and bad examples.**



Building A includes significant façade modulation combined with a corresponding roofline change to break up the building's perceived massing. Building B uses a substantial setback after the second floor and a noticeable change in fenestration pattern to help break up the massing.



Building C uses smaller scale articulation treatments, but when viewed from a distance they fail to effectively break up the larger scale massing of the structure. In building D, the white modulated component helps, but the predominant flat façade and roofline shown here would not meet the intent of these design guidelines.

3. DEPARTURES to Guidelines 1 and 2 above will be considered provided they meet the intent of the guidelines and the design criteria below. For example, a departure to Guideline B.1 may propose a design with only two articulation features instead of three or the articulation features exceed the maximum articulation interval.

- a. Consider the type and width of the proposed articulation/massing treatment and how effective it is in meeting the intent given the building's context.
- b. Consider the visibility of the façade. Less prominent side elevations warrant more flexibility than Tjerne Place frontages.

**Commented [BB17]:** These departure options are important – as its very hard to write prescriptive options for addressing these – the above are the most obvious ways – but there are so many creative ways that you can address this. The extra criteria language should help. If necessary we can include good examples of such departures.

- c. Consider the size and width of the building. Smaller buildings warrant greater flexibility than larger buildings.
  - d. For Guideline B.1, consider the quality of façade materials in concert with doors, windows, and other façade features and their ability to add visual interest to the street or internal site context from a pedestrian scale and more distant observable scales.
  - 4. Roofline modulation. Roofline modulation is not required on all buildings. However, it can be used as one of the façade articulation features in Guideline B.1 above. In order to qualify as an articulation feature, rooflines must employ one or more of the following:
    - a. For flat roofs or façades with horizontal eave, fascia, or parapet, the minimum vertical dimension of roofline modulation is the greater of two-feet or 0.1 multiplied by the wall height (finish grade to top of the wall) when combined with vertical building modulation techniques described in subsections above. Otherwise, the minimum vertical dimension of roofline modulation is the greater of four-feet or 0.2 multiplied by the wall height.
    - b. A pitched roofline or gabled roofline segment of at least 20-feet in width. Buildings with pitched roofs must include a minimum slope of 5:12 and feature modulated roofline components at the interval required per the applicable standard above.
    - c. A combination of the above.
- DEPARTURES will be considered provided the roofline modulation design effectively reduces the perceived scale of the building and adds visual interest.

**Figure 5.B.4**  
**Acceptable examples of roofline modulation.**



Roofline modulation examples for flat roofs.

**Figure 5.B.4**

**Acceptable examples of roofline modulation.**



The left building illustrates a pitched roof example and the right building includes a combination of flat and gabled rooflines.

**C. Architectural Elements**

We suggest a complete revision of this section as well – to include enhanced and more predictable provisions integrating design details and up to date building materials guidelines. Suggested new language:

I. Detail integration. All non-residential and mixed-use buildings must be enhanced with appropriate details. This standard applies to applicable building facades and other building elevations facing parks and containing primary building entrances. Such buildings must employ at least one detail element from each of the three categories below for each facade articulation interval (see subsection B.I above). For example, a building with 120-feet of lot frontage with a facade articulated at 30-foot intervals will need to meet the guidelines for each of the four facade segments below.

a. Window and/or entry treatment, such as:

- i. Display windows divided into a grid of multiple panes.
- ii. Transom windows.
- iii. Roll-up windows/doors.
- iv. Other distinctive window treatment that meets the intent of the guidelines.
- v. Recessed entry.
- vi. Decorative door.
- vii. Other decorative or specially designed entry treatment that meets the intent of the guidelines.

**Commented [BB18]:** Integrates a toolbox approach – a good balance of predictability and flexibility – ensures that some desirable details will be integrated into non-residential facades – particularly on the ground floor.

**Figure 5.C.1.a**

**Window and/or entry treatment examples.**

*TO ADD GOOD PHOTO EXAMPLES OF WINDOW AND/OR ENTRY TREATMENT EXAMPLES*

**b. Building elements and façade details, such as:**

- i. Custom-designed weather protection element such as a steel canopy, cloth awning, or retractable awning.**
- ii. Decorative building-mounted light fixtures.**
- iii. Bay windows, trellises, towers, and similar elements.**
- iv. Decorative, custom hanging sign(s) (option only available for building remodels).**
- v. Other details or elements that meet the intent of these guidelines.**

**Figure 5.C.1.b**

**Building elements and façade details examples.**

*TO ADD GOOD PHOTO EXAMPLES OF BUILDING ELEMENTS AND FAÇADE DETAILS EXAMPLES*

**c. Building materials and other façade elements, such as:**

- i. Use of decorative building materials/use of building materials. Examples include decorative use of brick, tile, or stonework.**
- ii. Artwork on building (such as a mural) or bas-relief sculpture.**
- iii. Decorative kick-plate, pilaster, base panel, or other similar feature.**
- iv. Hand-crafted material, such as special wrought iron or carved wood.**
- v. Other details that meet the intent of the guidelines.**

**Figure 5.C.1.c**

**Building materials and other façade element examples.**

*TO ADD GOOD PHOTO EXAMPLES OF BUILDING MATERIALS AND OTHER FAÇADE ELEMENT EXAMPLES*

**DEPARTURES for façade detail guidelines above will be considered provided the façade (at the overall scale and at the individual articulation scale) meets the intent of the guidelines.**



2. Window design guidelines.

- a. All windows (except storefront display windows) must employ designs that add depth and richness to the building façade. At least one of the following features must be included to meet this requirement:
  - a. Recess windows at least two-inches from the façade.
  - b. Incorporate window trim (at least three-inches wide) around windows
  - c. Incorporate other design treatments that add depth, richness, and visual interest to the façade.
- b. Highly reflective glass must not be used on more than ten-percent of a building façade or other building elevations facing parks and containing primary building entrances.

**Commented [BB19]:** Another important provision – as windows can often make the difference between a likeable and a strongly disliked building – from a community standpoint.

**Figure 5.C.2**

**Acceptable and unacceptable window design examples.**



The windows in Images A-C are recessed by at least two-inches from the façade. Images D and E feature a reveal/recess of less than two inches, but the contrasting frames and mullions effectively add a sense of depth and richness to the façade. The treatment in Image F does not effectively add a sense of depth and richness to the façade.

**Commented [BB20]:** I think this is useful – but if there is a concern that we’re being too prescriptive here – this is one item that may have less impact – particularly if a building meets all the other massing and articulation features. (it might meet this guideline nearly by default). But this adds another layer of protection. Note that the bad example below –wouldn’t meet the articulation guidelines – if it did – it wouldn’t be as bad – even if it didn’t have a cornice.

3. Cornice/roofline design. Buildings employing a flat roof must employ a distinctive roofline that effectively provides an identifiable “top” to the building. This could include a traditional cornice line or a contemporary interpretation of a traditional cornice line.

- a. Such rooflines must be proportional to the size and scale of the building.
- b. Understated cornice lines are permitted depending on the materials and design of the base and middle elements in reinforcing the base/middle/top configuration.

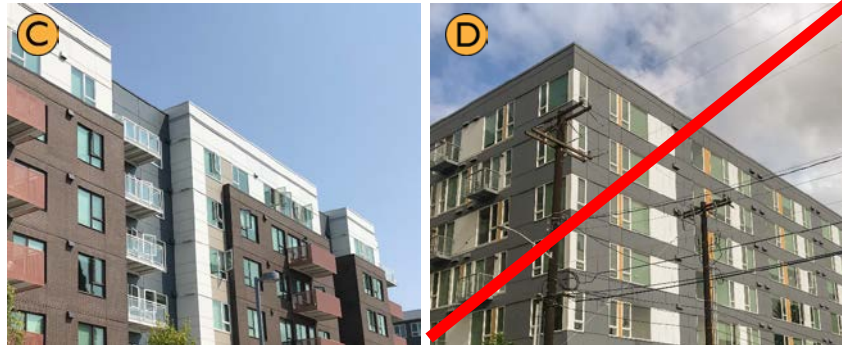
Figure 5.C.3 below illustrate acceptable and unacceptable examples.

**Figure 5.C.3**

**Examples of buildings employing a distinctive roofline that effectively provides an identifiable “top” to the building.**



Building A uses a traditional cornice along with upper level building at the right corner, and the left portion uses a very simple cornice line to complement the upper level setback. Building B uses a traditional cornice line along with pitched roof forms.



Building C uses a slight upper level modulation along with color and material change to create a distinctive roofline. Building D does not use any effective technique to distinguish the roofline.

- 4. Articulated building entries. The primary building entrance for an office building, hotel, apartment building, public or community-based facility or other multi-story commercial building must be designed as a clearly defined and demarcated standout architectural feature of the building. Such entrances must be easily distinguishable from regular storefront

**Commented [BB21]:** This is another good tool – strongly suggest keeping this.

entrances on the building. Such entries must be scaled proportional to the building. See Figure 5.C.4 below for good examples.

**Figure 5.C.4**

**Articulated building entry examples.**

TO ADD GOOD PHOTO EXAMPLES OF ARTICULATED BUILDING ENTRY EXAMPLES

5. Exterior materials.

*Here we suggest some more strategic changes and adding a lot of graphic support.*

- a. Use durable and high-quality materials must be employed. Shiny or highly reflective materials are not allowed. Materials should be those inspired by traditional - of typical use in the Northwest architecture, including:

Bevel or lap siding.

Rock, stone, and brick material.

Architectural shake-style roofing.

Metal roofs with standing seams.

- b. If sheet materials, such as composite fiber products or metal siding, are used as a siding material over more than 25 percent of a building's façade, use material with a matted finish in a muted color as specified in Color guidelines below. Include the following elements:

Visible window and door trim painted or finished in a complementary color.

Corner and edge trim that covers exposed edges of the siding material.

- c. If concrete blocks (concrete masonry units or "cinder blocks") are used for walls that are visible from a public street or park, use one or more of the following architectural treatments:

Use of textured blocks with surfaces such as split-face or grooved.

Use of colored mortar.

Use of other masonry types, such as brick, glass block, or tile, in conjunction with concrete blocks.

Other treatment methods approved by the City.

The applicant shall provide the City with samples of the material, proposed detail connections and a list of other project examples in the Puget Sound region that have used this application.

- d. Do not use the following materials in visible locations unless an exception is granted by the City:

Mirrored glass.

Corrugated fiberglass.

**Commented [BB22]:** Becomes increasingly difficult to make this statement

**Commented [BB23]:** Suggest breaking this up into separate sections – one for metal and one for hardi-panels – and apply the 25% tool for each. We can provide good/bad examples.

**Commented [BB24]:** I think this approach still works – though we might allow a departure opportunity for flexibility to integrate it in creative ways that meet the intent.

**Commented [BB25]:** This is largely good – but we can add good/bad photo examples.

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Chain-link fencing (with or without slats).

Synthetic materials with reflective surfaces, including galvanized steel and glossy vinyl siding.

Other treatment methods approved by the City.

- e. Paint all vents, gutters, downspouts, flashing, and electrical conduits to match the color of the adjacent surface unless they are being used expressly as a trim or accent element, or if the surface is made of an unpainted material such as brick.
- f. Provide approved address numbers so that they are legible to the public from the street fronting the property.

6. Colors

- a. Submit a color palette.
- b. Muted colors are encouraged for the background color of most buildings. A darker background color will allow the effective use of lighter colors for trim – where the highlights will show up better.
- c. Bright colors should generally be reserved for accents. Doors or special features may be painted a bright accent color.
- d. Bright luminescent or day-glow color are not allowed.

*Here's some similar alternative language I've used recently that I adjusted to possibly fit here. We'll want to discuss the use of "should" vs "shall" here to provide more teeth if we feels it's needed.*

Buildings should utilize exterior building colors that are inspired by traditional Northwest architectural themes. The primary building colors should be muted as a background element, to let the streetscape, natural building colors, facade articulation and ground level details create a rich and colorful setting. However, color should be carefully applied to help articulate facades, emphasize building details, and create visual interest and memorability to the downtown cityscape. Specifically:

- i. Exterior finish colors should be neutral shades of natural colors found in Northwest woodlands or colors typical of historic agrarian structures of the Northwest, and may include limited use of compatible accent colors.
- ii. Natural colors of wood, stone and brick should be emphasized.
- iii. Roof colors, trim, and accent colors should provide contrast to the primary background colors.
- iv. Colors which are alien to the surrounding natural environment are not appropriate. This includes man made, overly bright colors not typical of the natural environment in the Monroe area.
- v. For multi-building developments or other locations where buildings sit side by side, utilize a range of colors to provide contrast for added visual interest and avoid monotony.

7. Building equipment and service areas

*Suggested adjustments:*

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- *We'll suggest that visible service enclosures also include landscaping elements*
  - *Design/materials be compatible with the adjacent building(s).*
  - *For those service areas visible by adjacent multi-story buildings, that a roof/screen will be necessary overhead. We'll add photo examples.*
  - *We'll add location/design examples of utility meter integration*
  - *Add rooftop screening language/examples.*
- a. Building service elements and utility equipment should be contained within the building envelope, screened from public view, or on roofs where not visible to the public.
  - b. All on-site service areas, loading zones, outdoor storage areas (except outdoor retail sales areas under 100 square feet in occupied area), waste storage, disposal facilities, transformer and utility vaults, and similar activities shall be located in an area not visible from a public street, pedestrian connection, or open space. If this is not possible, then the service area, loading zone, storage area, or utility area must be screened from public view. Acceptable screening includes:

A masonry or wood enclosure incorporated into a building wall.

A solid hedge or other screening as approved by the City.

(Note: Visible chain link fencing with or without slats is not permitted.)

~~e. Service or utility areas or enclosures shall not be located in or be visible from public open space, including the Village Green and Focal Plaza.~~

## Chapter 6: Landscape Design

### A. Landscaped Plan Concept

Update the figure number reference in the second bullet to match with the existing graphic at end of subsection and suggest the following changes to the last bullet under subsection A. I and additional supporting images:

- Plantings and/or site features that soften the appearance of buildings, and enhance the building's architectural qualities.

**Figure 6.A.I**

**Utilizing landscaping to soften the appearance of buildings and enhancing their architectural qualities.**



Image A uses a colorful mix of plantings in terraces to soften the appearance of the building from the street and add visual interest. Image B uses plantings and architectural walls as a focal point in front of the building. Image C below shows an unacceptable example without any type of softening shrubs or plantings beyond a single deciduous tree and lawn area, creating a stark image that detracts from the building's character.

**Figure 6.A.1**

**Utilizing landscaping to soften the appearance of buildings and enhancing their architectural qualities.**



## **B. Street Landscaping**

Correct the guideline numbering associated with the page-break. Also, consider the following updates below:

2. Remove the cross-reference to Figure 8, which no longer exists.
3. Street trees placed in tree grates may be more desirable in storefront settings when on-street parking is present than planting strips in key pedestrian areas.
6. Update the cross-reference per other suggested changes above: Building Location & Orientation

## **C. Parking Lot Screening**

Consider the following updates:

Guidelines:

- ~~1.~~ Provide a landscaped drainage/stormwater treatment buffer between the side-walk/street and parking area where possible. Size the buffer as necessary to perform required stormwater treatment function ~~for a minimum of 20 feet is suggested. (See Figure 21.)~~ Otherwise, a 5-foot wide landscaping buffer consistent with Type III landscaping as specified in MMC, Chapter 18.78, to provide a see-through buffer between public streets and parking lots is required. Integrate a low wall (36-42 inches tall) built of concrete, masonry, or other durable materials at the back end of the planting strip; EXCEPTION: The landscape buffer must be 10 feet between parking areas and sidewalks along Chain Lake Road.



**Figure 6.C**

**Utilizing landscaping to soften the appearance of buildings and enhancing their architectural qualities.**

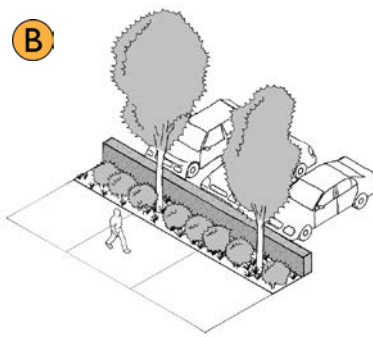


Image A is a good example of landscaped drainage/stormwater treatment buffer

#### **F. Screening High-Impact Uses**

*Is this section still necessary?*

### **Chapter 7: Signage and Lighting**

*A complete update of this chapter is warranted given the citywide sign code was updated in 2019. The content below focuses on design elements appropriate for North Kelsey that are not necessarily addressed in the citywide code (Chapter 22.50).*

#### **A. Signs**

*A complete update of this section is warranted given the citywide sign code was updated in 2019. The content below focuses on design elements appropriate for North Kelsey that are not necessarily addressed in the citywide code (Chapter 22.50).*





##### **Intent:**




- To encourage signage that is both clear and of appropriate scale for the project.
- To enhance the visual qualities of signage through the use of complementary sizes, shapes, colors, and methods of illumination.
- To provide a comprehensive sign program that creates consistent design criteria for the entire North Kelsey planning area.



**Guidelines:**

- I. Permitted sign illumination types. Table 7.A.I below specifies permitted sign illumination types by zone.

<b>Table 7.A.I</b> <b>Permitted signs illumination types.</b>			
<b>Illumination Type</b>		<b>Permitted?</b>	<b>Other requirements</b>
Channel letter. Light is emitted through the front or face of the letters.		Yes	May be incorporated into a permitted wall, pole, or monument sign
Halo illumination. Letter faces are opaque and light source provides halo effect through backlighting.		Yes	May be incorporated into a permitted wall, pole, or monument sign
Push-through. Letters are cut out of opaque sign face. Interior light shines through letter faces only.		Yes	May be incorporated into a permitted wall, pole, or monument sign
Neon.		Yes	May be incorporated into a permitted wall, projecting, window, pole, or monument sign

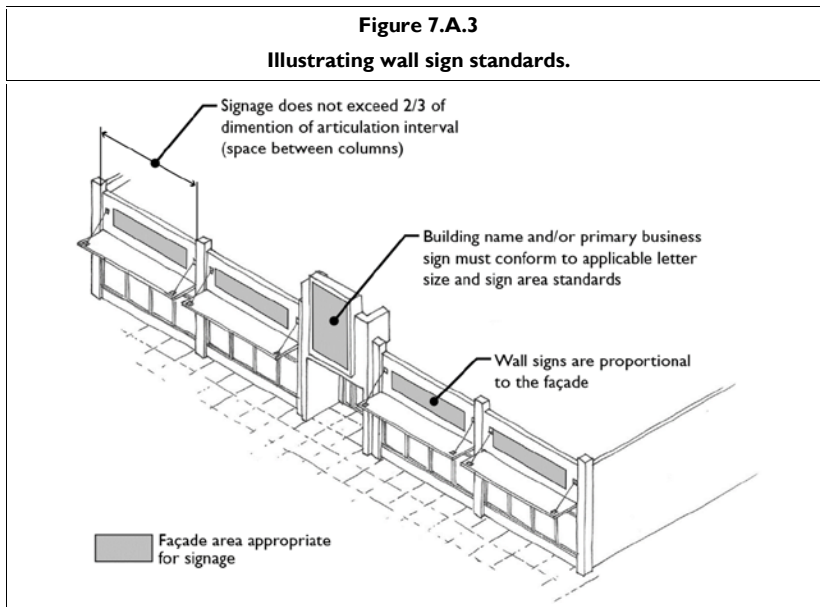
<b>Table 7.A.1</b> <b>Permitted signs illumination types.</b>			
<b>Illumination Type</b>		<b>Permitted?</b>	<b>Other requirements</b>
Externally-illuminated sign.		Yes	Illumination techniques must focus the light on the sign and avoid glare to the sky, streets, sidewalks, and other public spaces, and adjacent uses.
Internally-illuminated cabinet signs. Sign face is illuminated through translucent casing. This includes internally illuminated changeable copy signs.		No	
Internally-illuminated awning signs. Awning face is illuminated through awning material.		No	
Electronic and video display signs. A sign which contains electronically-operated moving parts or which flashes or simulates motion by the use of electric lights.		No	

2. Signs projection over right-of-way. Only those projecting and suspended and under awning signs may be permitted to project into the public right-of-way, provided they meet all requirements relating to traffic, construction, safety and size, and are attached to an approved awning, canopy, marquee or porte cochere.
3. Wall sign location and design.
  - a. Wall signs must be proportional to the façade and are limited to 2/3 of individual façade width dimension. This standard also applies to upper level businesses.
  - b. Wall signs may not cover windows, building trim, an existing building name sign, or special ornamentation features. Preferred areas for installation of wall signs include

blank areas above marquees, areas between vertical piers or columns, blank areas on a gabled roof, or upper reaches of a false fronted building.

- c. Stacked words on wall signs are permitted. Generally, the primary business name is encouraged to be provided on one line, with additional text on rows above and/or below providing supporting information about the business in smaller fonts.

**Figure 7.A.3**  
**Illustrating wall sign standards.**



## **B. Lighting**

*Most of the provisions here are duplicative of Chapter 15.15. Suggest keeping only those provisions noted below.*

### **Intent:**

- To enhance and encourage evening activities.
- To provide a distinctive character to the area.

### **Guidelines:**

1. Uplighting on trees and provisions for seasonal lighting are encouraged.
2. Accent lighting on architectural and landscape features is encouraged to add interest and focal points.

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3. Pedestrian-scaled lighting is required within the planned development area along all streets and in all public plazas and courts. Pedestrian-scaled lighting fixtures are generally 12-14 feet and of a character complementary to the building architecture.

**22.26.030 North Kelsey/Tjerne Place Overlay District.**

All properties located within the North Kelsey/Tjerne Place Overlay District (NK/TP-O) shall have a base zoning designation of General Commercial. The North Kelsey/Tjerne Place Overlay District consists of approximately one hundred acres of land located north of SR 2, south of the proposed SR 2 bypass, east of the SR 522 overpass, and west of Chain Lake Road.

A. Design Guidelines. All development within the North Kelsey/Tjerne Place Overlay District shall comply with the North Kelsey Design Guidelines in addition to the applicable requirements of MMC Chapter 22.42, Design Standards.

~~B. Mixed Use Development. The purpose of allowing mixed use is to place attached, high density housing in close proximity to goods and services. Vertical and horizontal mixed use developments are allowed in the North Kelsey/Tjerne Place Overlay District and shall comply with the following:~~

~~1. Vertical mixed use incorporates commercial and multifamily residential uses in one structure.~~

~~a. A minimum of fifty (50) percent of the ground floor must be dedicated to a commercial use.~~

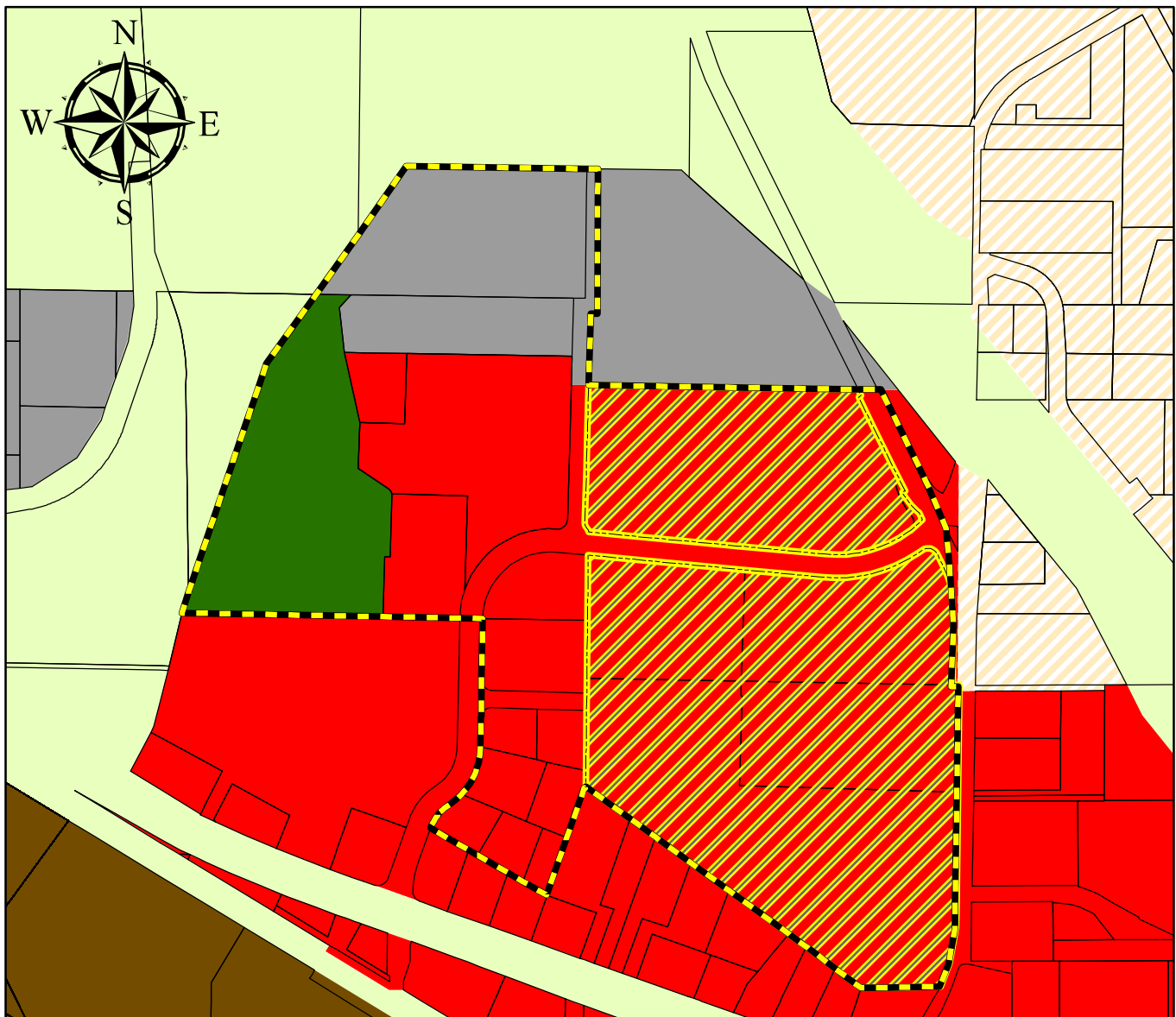
~~b. The minimum density shall be 26 units per gross acre.~~

~~2. Horizontal mixed use incorporates a mix of structures independently containing multifamily residential and commercial uses.~~

~~a. The mixed use development shall be contained within one parcel or contiguous parcels under one ownership.~~

~~b. A minimum of fifty (50) percent of the gross usable lot(s) area shall be for commercial development. The area of a lot shall be developed at a minimum of twenty (20) percent ground floor commercial. A document, in a form acceptable to the City Attorney, shall be recorded against the property restricting residential development on the portion of the property designated commercial.~~

~~c. The minimum density shall be 26 units per gross acre.~~



## Current Boundaries and Comprehensive Plan Designations

### Features and Boundaries

#### Land Use Designations

- (GC) - General Commercial
- (I) - Industrial
- (P/O) - Parks / Open Space
- (SRU) - Special Regional Use
- (R2-5) - Dwellings Per Acre
- (R8-11) - Dwellings Per Acre



North Kelsey Planning Area



North Kelsey Planned Development Area



ZONING MAP

Zoning Districts

Residential Use

Single-Family Residential - 4 Units per Acre (R4)

Single-Family Residential - 7 Units per Acre (R7)

Single-Family Residential - 15 Units per Acre (R15)

Multifamily Residential (R25)

Mixed Use

Mixed Use - Neighborhood (MN)

Mixed Use - Medical (MM)

Mixed Use - General (MG)

Commercial Use

Tourist Commercial (TC)

Downtown Commercial (DC)

General Commercial (GC)

Industrial Transition (IT)

Public Facility Use

Institutional (IN)

Transportation (TR)

Open Space Use

Limited Open Space (LS)

Parks (P)

Industrial Use

Shoreline Industrial (SI)

Light Industrial (LI)

General Industrial (GI)

Zoning Overlay Districts

Airport Compatibility Overlay

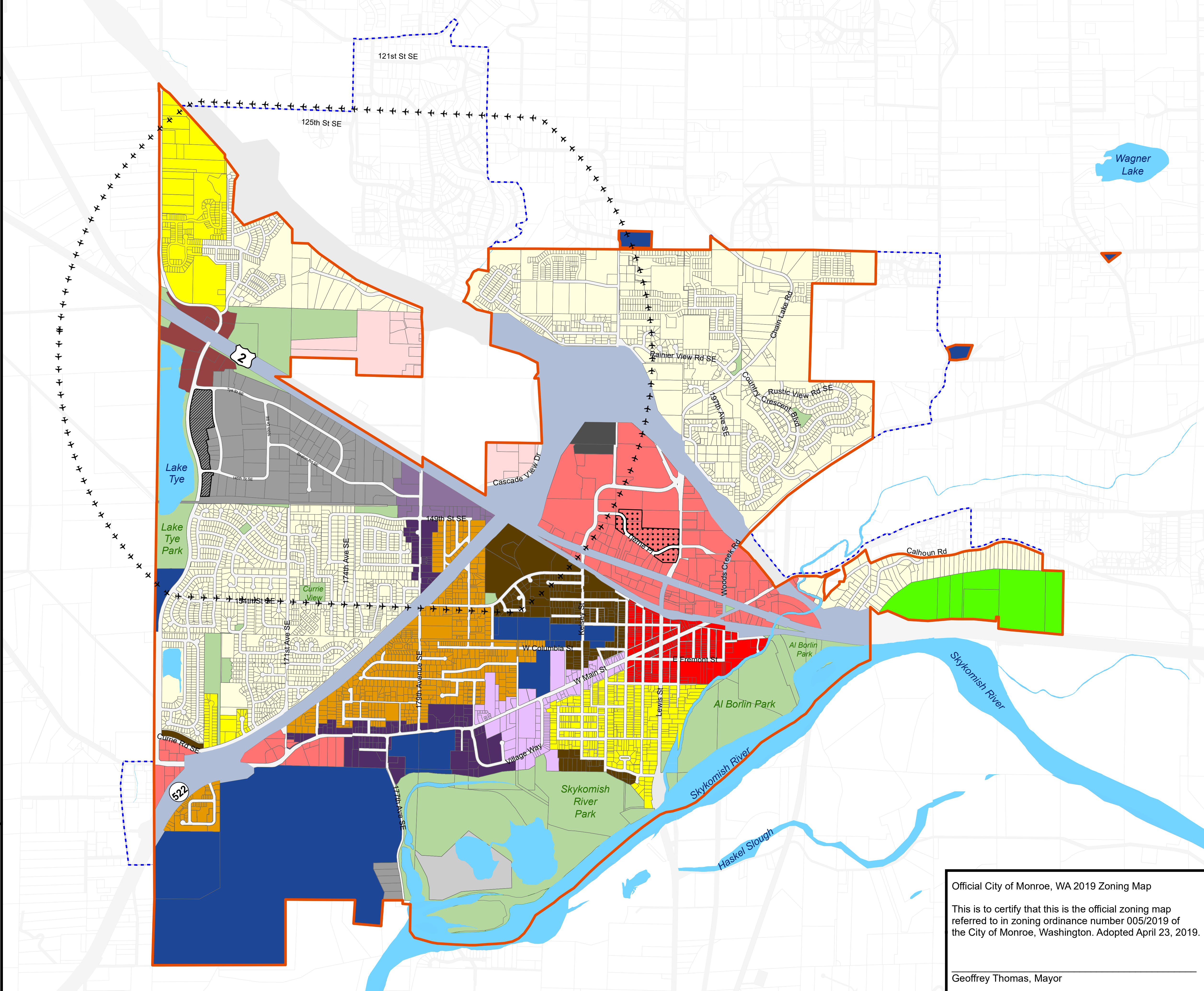
Fryelands Commercial Overlay (FC-O)

North Kelsey/Tjerne Place Overlay (NK/TP-O)

Boundaries

Monroe City Limits

Urban Growth Area



Official City of Monroe, WA 2019 Zoning Map

This is to certify that this is the official zoning map referred to in zoning ordinance number 005/2019 of the City of Monroe, Washington. Adopted April 23, 2019.

Geoffrey Thomas, Mayor





## SEPA ENVIRONMENTAL CHECKLIST

### ***Purpose of checklist:***

Governmental agencies use this checklist to help determine whether the environmental impacts of your proposal are significant. This information is also helpful to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts or if an environmental impact statement will be prepared to further analyze the proposal.

### ***Instructions for applicants:***

This environmental checklist asks you to describe some basic information about your proposal. Please answer each question accurately and carefully, to the best of your knowledge. You may need to consult with an agency specialist or private consultant for some questions. You may use "not applicable" or "does not apply" only when you can explain why it does not apply and not when the answer is unknown. You may also attach or incorporate by reference additional studies reports. Complete and accurate answers to these questions often avoid delays with the SEPA process as well as later in the decision-making process.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will help describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

### ***Instructions for Lead Agencies:***

Please adjust the format of this template as needed. Additional information may be necessary to evaluate the existing environment, all interrelated aspects of the proposal and an analysis of adverse impacts. The checklist is considered the first but not necessarily the only source of information needed to make an adequate threshold determination. Once a threshold determination is made, the lead agency is responsible for the completeness and accuracy of the checklist and other supporting documents.

### ***Use of checklist for nonproject proposals:***

For nonproject proposals (such as ordinances, regulations, plans and programs), complete the applicable parts of sections A and B plus the SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS (part D). Please completely answer all questions that apply and note that the words "project," "applicant," and "property or site" should be read as "proposal," "proponent," and "affected geographic area," respectively. The lead agency may exclude (for non-projects) questions in Part B - Environmental Elements –that do not contribute meaningfully to the analysis of the proposal.



## **A. Background**

1. Name of proposed project, if applicable:

*North Kelsey/Tjerne Place Overlay District Amendments to Comprehensive Plan and Monroe Municipal Code to allow single-purpose multi-family developments.*

2. Name of applicant:

*City of Monroe*

3. Address and phone number of applicant and contact person:

*Ben Swanson, Community Development Director  
City of Monroe  
806 West Main Street, Monroe, Washington 98272  
360-863-4544  
BSwanson@monroewa.gov*

4. Date checklist prepared:

*July 16, 2020*

5. Agency requesting checklist:

*City of Monroe, Department of Community Development*

6. Proposed timing or schedule (including phasing, if applicable):

*The project will be subject to the following steps. An exact schedule has not been established.*

- *Planning Commission review*
- *Planning commission public hearing*
- *Department of Commerce mandatory 60-day review period*
- *Adoption of Comprehensive Plan Amendment by City Council*

7. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain.

*This proposal is non-project action that would change the Title 22 of the Monroe Municipal Code (MMC). It is expected that one or more development applications will follow adoption of the proposed regulations.*

8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.

*City of Monroe North Kelsey Sub Area Plan, Planned Action Final Supplemental Environmental Impact Statement, March 10, 2004*

*North Kelsey Sub-Area Trip Generation Analysis Memorandum, prepared by Fehr & Peers, dated June 27, 2018*

**Background**

*In 2004 the City of Monroe (City) adopted a Planned Action Ordinance for a 68-acre site that included mixed-use commercial and residential development of the North Kelsey Sub Area. These actions were evaluated in the North Kelsey Supplemental Environmental Impact Statement (SEIS) (City of Monroe 2004). Since 2004, large format retail, a shopping center, a clinic and medical offices, and a fast food restaurant have been constructed in the North Kelsey Sub Area.*

*The City is currently considering a proposal for the remaining undeveloped portion of the North Kelsey Sub Area to allow construction of approximately 200 units of multifamily housing accompanied by 54,000 square feet of retail with the commercial development in a separate building or buildings (See Attachment 1).*

9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain.

*As of the date of this writing, no other proposal applications are pending government approvals that affect the North Kelsey Sub Area.*

10. List any government approvals or permits that will be needed for your proposal, if known.

- *Comprehensive Plan Amendment*
- *Ordinance adopting changes to MMC*
- *Washington State Department of Commerce review process*

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. (Lead agencies may modify this form to include additional specific information on project description.)

*The proposal is to change the use and development standards for the North Kelsey/Tjerne Place Overlay District (NK/TP-O) to allow multifamily development that is not in a mixed-use building. Under current zoning, multifamily residential units would only be allowed in a mixed-use type of structure, with both commercial and multifamily in the same structure. The proposed change would allow single purpose or stand-alone multifamily buildings, but would not increase the allowable density of either commercial or residential development.*

*While this is a non-project action, it pertains to the development of a specific site in Monroe, Washington. The affected geographic area includes the entire North Kelsey Sub Area, but because other parcels in that area have been recently redeveloped, the primary effect is on the remaining six undeveloped parcels of the North Kelsey Sub Area.*

*The City is currently considering a proposal for the remaining undeveloped portion of the North Kelsey Sub Area to allow construction of approximately 200 units of multifamily housing accompanied by 54,000 square feet of retail with the commercial development in a separate building or buildings (Attachment 1). Because this is the likely density of development that could occur under the proposed change in regulations, this analysis uses that proposed development as a reasonable approximation of the impacts that could result from this proposed change in regulations. However, any project built on the site would be subject to project level SEPA review.*

12. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township, and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist.

*The affected geographic area includes the entire North Kelsey Sub Area, but because other parcels in that area have been recently redeveloped, the primary effect is on the remaining six undeveloped parcels of the North Kelsey Sub Area. Those parcels are north of Tjerne Place SE, west of Chain Lake Road, and south of North Kelsey Street in Monroe, Washington. Parcels 27060100115000 and 27060100115100 are located south, adjacent to Providence Health; parcels 27060100115200, 027060100115300, 27060100115500, and 27060100115400 are located approximately 400 feet south of Lowe's Home Improvement (Attachment 1). These parcels are identified in the North Kelsey Development Plan as the southern portion of the "North Kelsey South Area".*

## **B. Environmental Elements**

### **1. Earth**

- a. General description of the site:

*The affected geographic area is characterized by generally very flat topography and was graded to its current configuration during previous development.*

(circle one): Flat, rolling, hilly, steep slopes, mountainous, other:

- b. What is the steepest slope on the site (approximate percent slope)?

*The affected geographic area is flat; steepest slopes are less than 3%.*

- c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils.

*The soils in the affected geographic area are predominantly Everett very gravelly sandy loam, 0 to 8 percent slopes (NRCS 2020).*

- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe.

*No, there are no indications or history of unstable soils.*

- e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill.

*This is a non-project level SEPA Checklist and no filling, excavation or grading is proposed. These types of activities will be evaluated in a project-level SEPA for a specific project.*

- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.

*This is a non-project level SEPA Checklist and no filling, excavation or grading is proposed. Individual projects will be required to comply with the policies and regulations of the MMC.*

- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)?

*The affected geographic area is approximately 10% impervious surfaces. The majority is undeveloped.*

- h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any:

*This is a non-project level SEPA Checklist, measures to reduce or control erosion, or other earth impacts will be evaluated in a project level SEPA evaluation for a specific project. Individual projects will be required to comply with the policies and regulations of the MMC.*

## **2. Air**

- a. What types of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known.

*This is a non-project level SEPA Checklist, and no activities that would result in air impacts are proposed. Impacts to the air from a specific project will be evaluated in a project level SEPA evaluation and will be required to comply with the policies and regulations of the MMC.*

- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe.

*This is a non-project level SEPA Checklist, and emissions or odors would not affect the proposal. Emissions or odors associated with Lakeside Industries, an asphalt plant, may affect future proposed residential developments. Potential impacts were evaluated in the North Kelsey SEIS (City of Monroe 2004). Lakeside Industries operates according to federal and state laws that regulate the types and levels of emissions considered to be acceptable. Potential impacts will be evaluated for a specific project in a project level SEPA evaluation.*

- c. Proposed measures to reduce or control emissions or other impacts to air, if any:

*This is a non-project level SEPA Checklist, measures to reduce or control emissions, or other impacts to air will be evaluated in a project level SEPA evaluation for a specific project.*

### **3. Water**

- a. Surface Water:

- 1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.

*No, there are no known water bodies in the vicinity of the affected geographic area. There are two constructed stormwater ponds in the vicinity.*

- 2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.  
*NOT APPLICABLE.*

- 3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.  
*NOT APPLICABLE.*

- 4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.  
*NOT APPLICABLE.*

- 5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan.

*No.*

- 6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

*This is a non-project level SEPA Checklist and no discharge of waste material is proposed. These types of activities will be evaluated in a project level SEPA for a specific project. Individual projects will be required to comply with the policies and regulations of the MMC.*

b. Ground Water:

- 1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known.

*This is a non-project level SEPA Checklist and no withdrawal from a well or discharge to groundwater is proposed. These types of activities will be evaluated in a project level SEPA for a specific project. Individual projects will be required to comply with the policies and regulations of the MMC.*

- 2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals. . . ; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve.

*This is a non-project level SEPA Checklist and no discharge to the ground is proposed. These types of activities will be evaluated in a project level SEPA for a specific project. Individual projects will be required to comply with the policies and regulations of the MMC.*

c. Water runoff (including stormwater):

- 1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

*This is a non-project level SEPA Checklist and no sources of runoff proposed. These types of activities will be evaluated in a project level SEPA for a specific project. Individual projects will be required to comply with the policies and regulations of the MMC.*

- 2) Could waste materials enter ground or surface waters? If so, generally describe.

*NOT APPLICABLE.*

- 3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe.

*NOT APPLICABLE.*

d. Proposed measures to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any:

*NOT APPLICABLE.*

#### 4. Plants

- a. Check the types of vegetation found on the site:

☒ deciduous tree: alder, maple, aspen, other

☐ evergreen tree: fir, cedar, pine, other

☐ shrubs

☒ grass

☐ pasture

☐ crop or grain

☐ Orchards, vineyards or other permanent crops.

☐ wet soil plants: cattail, buttercup, bullrush, skunk cabbage, other

☐ water plants: water lily, eelgrass, milfoil, other

☐ other types of vegetation

- b. What kind and amount of vegetation will be removed or altered?

*This is a non-project level SEPA Checklist and no vegetation removal or alternation is proposed. These types of activities will be evaluated in a project level SEPA for a specific project.*

- c. List threatened and endangered species known to be on or near the site.

*No threatened or endangered species are known to be on or near the site (WDNR 2020).*

- d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:

*NOT APPLICABLE.*

- e. List all noxious weeds and invasive species known to be on or near the site.

*There are no known noxious weeds or invasive species known to be on or near the affected geographic area.*

#### 5. Animals

- a. List any birds and other animals which have been observed on or near the site or are known to be on or near the site.

*Animals likely to occur are typical urban animals and birds.*

*Fish: not applicable*

*Amphibians: none known*

*Reptiles: none known*

*Birds: species adapted to urban areas such as gulls, American crow, rock pigeon, chickadee, robin, Steller's jay.*

*Mammals: species adapted to urban areas such as Norway rat, raccoon, opossum.*

b. List any threatened and endangered species known to be on or near the site.

*No threatened or endangered species are known to be on or near the affected geographic area (WDFW 2020).*

c. Is the site part of a migration route? If so, explain.

*The project affected geographic area is located within the Pacific Flyway, which is a flight corridor for migrating waterfowl and other avian fauna. The Pacific Flyway extends from Alaska south to Mexico and South America. No portion of the proposed project would interfere with or alter the Pacific Flyway.*

d. Proposed measures to preserve or enhance wildlife, if any:  
*NOT APPLICABLE.*

e. List any invasive animal species known to be on or near the site.

*There are no known invasive animals species on or near the affected geographic area.*

## **6. Energy and Natural Resources**

a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.

*This is a non-project level SEPA Checklist and energy will not be needed. Energy needs will be evaluated as part of individual projects.*

b. Would your project affect the potential use of solar energy by adjacent properties?  
If so, generally describe.  
*NOT APPLICABLE.*

c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any:  
*These types of activities will be evaluated in a project level SEPA for a specific project. Individual projects will be required to comply with the policies and regulations of the MMC.*



## 7. Environmental Health

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe.

*This is a non-project level SEPA Checklist and there would not be environmental health hazards as a result of the proposal. These types of activities will be evaluated in a project level SEPA for a specific project and be required to comply with the policies and regulation of the MMC.*

- 1) Describe any known or possible contamination at the site from present or past uses.

*There are no known contaminations at the affected geographic area. Lakeside Industries Mining previously operated on the North Kelsey North Site, just north of the affected geographic area (Ecology 2020).*

- 2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity.

*None known.*

- 3) Describe any toxic or hazardous chemicals that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project.

*This is a non-project level SEPA Checklist and there would be no hazardous chemicals stored or used as a result of the proposal. These types of activities will be evaluated in a project level SEPA for a specific project and be required to comply with the policies and regulation of the MMC.*

- 4) Describe special emergency services that might be required.

*This is a non-project level SEPA Checklist and there would be no special emergency services required as a result of the proposal.*

- 5) Proposed measures to reduce or control environmental health hazards, if any:  
*NOT APPLICABLE.*

### b. Noise

- 1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?

*For the North Kelsey SEIS (City of Monroe 2004), Noise analysis was conducted that focused on noise from the Lakeside Industries operations (conveyor belts moving gravel and the truck traffic), previously located on the property now occupied by Walmart. The noise analysis, detailed in the SEIS, determined that the noise impacts were not significant and would not create adverse impacts (City of Monroe 2004). Lakeside Industries has moved to north of the Galaxy Theater in 2006-2007, which is further from the affected geographic area.*

2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.

*This is a non-project level SEPA Checklist and there would be no noise associated with the proposal. These types of activities will be evaluated in a project level SEPA for a specific project and be required to comply with the policies and regulation of the MMC.*

3) Proposed measures to reduce or control noise impacts, if any:  
*NOT APPLICABLE.*

## **8. Land and Shoreline Use**

a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe.

*The affected geographic area is currently undeveloped. Adjacent properties are commercial uses, a medical center, restaurants, and hotels. The proposal would change the zoning to allow stand alone multi-family residential, however, the number of residences is not greater than was evaluated in the North Kelsey SEIS (City of Monroe 2004). Thus the proposal is not expected to affect adjacent land uses.*

b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use?

*Farming and forestry have occurred historically in the area. It is not known if the affected geographic area was a working farm or forest.*

1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how:

*No.*

c. Describe any structures on the site.

None.

- d. Will any structures be demolished? If so, what?

No.

- e. What is the current zoning classification of the site?

*General Commercial (GC) - North Kelsey/Tjerne Place Overlay District (NK/TP-O)*

- f. What is the current comprehensive plan designation of the site?

*General Commercial (GC)*

- g. If applicable, what is the current shoreline master program designation of the site?

*NOT APPLICABLE.*

- h. Has any part of the site been classified as a critical area by the city or county? If so, specify.

No.

- i. Approximately how many people would reside or work in the completed project?

*This is a non-project level SEPA Checklist and there would be no residents associated with the proposal. Individual projects will be required to comply with the policies and regulations of the MMC. The proposal would change zoning allowing stand alone multifamily residential development in the North Kelsey/Tjerne Place Overlay District; however, the number of residences allowed would not increase.*

- j. Approximately how many people would the completed project displace?

None.

- k. Proposed measures to avoid or reduce displacement impacts, if any:

*NOT APPLICABLE.*

- l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:

*NOT APPLICABLE.*

- m. Proposed measures to reduce or control impacts to agricultural and forest lands of long-term commercial significance, if any:

NOT APPLICABLE.

## **9. Housing**

- a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing.

*This is a non-project level SEPA Checklist and there would be no residences constructed directly as a result of the change in use and development standards. The proposal would facilitate residential development of approximately 200 units on the undeveloped portion of the North Kelsey Sub Area. Individual projects will be required to undergo a project level SEPA review and comply with the policies and regulations of the MMC. It is not known what income level the housing would be targeted toward.*

- b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing.

*None.*

- c. Proposed measures to reduce or control housing impacts, if any:  
NOT APPLICABLE.

## **10. Aesthetics**

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?

*This is a non-project level SEPA Checklist and there would be no structures constructed as a result of the change in zoning to allow stand-alone multifamily development. Structure height would be evaluated in a project level evaluation and would be required to comply with the North Kelsey Development Pan Design Guidelines (2012, or as amended) and MMC.*

- b. What views in the immediate vicinity would be altered or obstructed?

*This is a non-project level SEPA Checklist and there would be no changes to view associated with the proposal. Changes to views would be evaluated in a project level evaluation and would be required to comply with the North Kelsey Development Pan Design Guidelines (2012) and MMC.*

- c. Proposed measures to reduce or control aesthetic impacts, if any:  
NOT APPLICABLE.

## **11. Light and Glare**

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur?

*This is a non-project level SEPA Checklist and there would be no light or glare associated with the proposal. Light and glare would be evaluated in a project level evaluation and would be required to comply with the North Kelsey Development Pan Design Guidelines (2012) and MMC.*

- b. Could light or glare from the finished project be a safety hazard or interfere with views?

**NOT APPLICABLE.**

- c. What existing off-site sources of light or glare may affect your proposal?

*Off-site sources of light or glare include parking lot and building lighting from surrounding commercial properties. Light and glare from traffic and lighting on Highway 2 may also affect the proposal. Potential effects would be evaluated in a project level SEPA for a specific development proposal.*

- d. Proposed measures to reduce or control light and glare impacts, if any:

**NOT APPLICABLE.**

## **12. Recreation**

- a. What designated and informal recreational opportunities are in the immediate vicinity?

*The affected geographic area is surrounded by commercial development and recreational opportunities in the immediate vicinity are quite limited.*

- b. Would the proposed project displace any existing recreational uses? If so, describe.

*This is a non-project level SEPA Checklist and there would be no changes to recreation. Potential impacts to recreation will be evaluated in a project level SEPA for a specific project and be required to comply with the policies and regulation of the MMC.*

- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any:

**NOT APPLICABLE.**

## **13. Historic and cultural preservation**

- a. Are there any buildings, structures, or sites, located on or near the site that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers? If so, specifically describe.

*There are no known buildings, structures, or sites, located on or near the site that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers. Historic and cultural resources were not identified as an element of concern in the North Kelsey SEIS (City of Monroe 2004). Historic and cultural resources will be evaluated for a specific project application.*

- b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources.

*No.*

- c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archeology and historic preservation, archaeological surveys, historic maps, GIS data, etc.

*This is a non-project level SEPA Checklist and no land disturbance is proposed. Specific projects will be evaluated in a project level SEPA evaluation and required to comply with the policies and regulation of the MMC.*

- d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required.

*This is a non-project level SEPA Checklist and no land disturbance is proposed.*

#### **14. Transportation**

- a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any.

*Public streets serving the affected geographic area are Tjerne Place SE to the south, Chain Lake Road to the east, and North Kelsey Street to the north. US-2 also runs east west south of the affected geographic area.*

- b. Is the site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop?

*Transit to Monroe is provided by Community Transit. Buses 270, 271 and 424 stop at the Monroe Park and Ride which is approximately 1.3 miles from the affected geographic area. Buses 270 and 271 also stop at Highway 2 and Woods Creek Rd approximately 0.3 miles from the affected geographic area.*

- c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate?

*This is a non-project level SEPA Checklist and there would be no parking changes associated with the proposal. Parking would be evaluated in a project level evaluation and would be required to comply with MMC.*

- d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private).

*This is a non-project level SEPA Checklist and there would be no transportation facility changes associated with the proposal. Transportation impacts would be evaluated in a project level evaluation and would be required to comply with MMC.*

- e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe.

No.

- f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and nonpassenger vehicles). What data or transportation models were used to make these estimates?

*This is a non-project level SEPA Checklist and there would be no changes to vehicle trips associated with the proposal. Peak hour trip generation was evaluated for the proposed development on the undeveloped parcels, to compare with the evaluation in the North Kelsey Sub Area Planned Action Ordinance. That analysis found the development would generate approximately 300 to 325 trips during the PM peak hour, the highest volume period during the day. This was found to be less than anticipated when considered together with all development that has occurred under the Planned Action.*

*Vehicle trips would be evaluated in a project level evaluation and would be required to comply with MMC.*

- g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, generally describe.

No.

- h. Proposed measures to reduce or control transportation impacts, if any:

NOT APPLICABLE.

## **15. Public Services**

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe.

*This is a non-project level SEPA Checklist and there would be no increased need for public services associated with the proposal. Impacts to public services would be evaluated at the project level.*

- b. Proposed measures to reduce or control direct impacts on public services, if any.  
**NOT APPLICABLE.**

## **16. Utilities**

- a. Circle utilities currently available at the site:

electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system,

other \_\_\_\_\_

*The affected geographic area is currently undeveloped, but all necessary utilities are available at the site, or will be extended as needed with a development proposal.*

- d. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed.

*This is a non-project level SEPA Checklist and there are no utilities proposed with the change in use and development standards. Impacts to public services would be evaluated at the project level.*

## **C. Signature**

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature: \_\_\_\_\_

Name of signee Mark S Johnson

Position and Agency/Organization: Environmental Science Associates, on behalf of the  
City of Monroe.

Date Submitted: July 16, 2020



NOTE: SEE SECTION D BELOW

## D. Supplemental sheet for non-project actions

(IT IS NOT NECESSARY to use this sheet for project actions)

Because these questions are very general, it may be helpful to read them in conjunction with the list of the elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise?

*As a non-project action, revising the comprehensive plan and use and development regulations within the affected geographic area would not increase discharges to water; emissions to air; production of noise; production, storage, or releases of toxic or hazardous substances. The North Kelsey Sub Area Plan included multi-family residential and the North Kelsey Sub Area Plan SEIS (City of Monroe 2004) included evaluation of multi-family residential in potential mixed-use developments. The following potential impacts were identified in the North Kelsey Sub Area Plan SEIS (City of Monroe 2004), and are not likely to increase a result of the proposal:*

- **Discharges to Water:** Newly created impervious surface could result in contamination of surface water, streams, rivers, and ground water resources by vehicles by-products and industrial operations. Untreated surface water runoff could include increased sedimentation loads, and water mixed with vehicle oils and heavy metals. To avoid impacts of these pollutants, a surface water management facility was required. Stormwater ponds were constructed to collect stormwater from Chainlake Road and Tjerne Place and the proposed courtyard area in the original sub area plan. Any new development will have to install private stormwater facilities.
- **Emissions to Air.** Commercial and residential development was not expected to create significant air quality impacts. An evaluation of traffic from the anticipated types of development showed that traffic impacts would be the same or less, therefore air emissions would similarly be no greater than found in the SEIS.
- **Production of Noise.** Development scenarios were not expected to generate significant adverse noise impacts.
- **Production, Storage, or Release of Toxic or Hazardous Substances.** No significant impacts were anticipated.

2. How would the proposal be likely to affect plants, animals, fish, or marine life?

*Impacts to plants, animal, fish or marine life are not expected. Potential impacts to plants or animals from a stand-alone Multi-family residential development would be the same or less than for a mixed use development. The number of occupants and level of activity would be the same or less. Additionally, the affected geographic area is an undeveloped grass area with limited habitat. Development would comply with applicable MMC as well as the North Kelsey Development Plan Design Guidelines (City of Monroe 2012).*

Proposed measures to protect or conserve plants, animals, fish, or marine life are:

*Development would comply with applicable MMC as well as the North Kelsey Development Plan Design Guidelines (City of Monroe 2012, as amended).*

3. How would the proposal be likely to deplete energy or natural resources?

*Energy demand for multi-family development would be the same or less than for a general commercial type development.*

Proposed measures to protect or conserve energy and natural resources are:

*Development would comply with applicable MMC as well as the North Kelsey Development Plan Design Guidelines (City of Monroe 2012 as amended).*

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection; such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, floodplains, or prime farmlands?

*There are no known environmentally sensitive areas parks, wilderness, wild and scenic rivers, threatened or endangered species, historic or cultural sites, wetlands, floodplains, or prime farmland in the immediate vicinity of the affected geographic area.*

Proposed measures to protect such resources or to avoid or reduce impacts are:

*None required.*

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?

*The affected geographic area is not in the shoreline. The proposal to allow stand-alone multi-family development would be compatible with existing uses in the affected geography. While generally compatible with the Comprehensive Plan and Sub Area plan, these would be amended to clarify that these types of uses are considered compatible.*

Proposed measures to avoid or reduce shoreline and land use impacts are:

*Development would comply with applicable MMC as well as the North Kelsey Development Plan Design Guidelines (City of Monroe 2012 as amended).*

6. How would the proposal be likely to increase demands on transportation or public services and utilities?

*The number of occupants and level of activity expected with multi-family residential would be the same or less than with mixed use development, therefore the demand for public services and utilities would not increase. A Trip Generation Analysis was conducted in 2018 (Fehr & Peers) that evaluated the potential number of trips for a multi-family development and compared it to trips evaluated in the North Kelsey Sub Area SEIS (City of Monroe 2004). The study showed that a multi-family development would fall within the range of capacity defined in the SEIS. The SEIS found the impacts would not be significant if transportation improvements were developed per the plan. All roads proposed in the plan have been developed already.*

Proposed measures to reduce or respond to such demand(s) are:

*None. Development will be required to comply with MMC impact fees and other requirements.*

7. Identify, if possible, whether the proposal may conflict with local, state, or federal laws or requirements for the protection of the environment.

*No conflicts are anticipated with any local, state or federal laws or requirements protection the environment. Any development would be subject to compliance with the MMC. The proposed changes do not supersede other agencies' authority to regulate projects within their jurisdiction (e.g., U.S. Army Corps of Engineers, Washington Department of Fishing and Wildlife, Washington Department of Ecology, Washington Department of Natural Resources, etc.).*

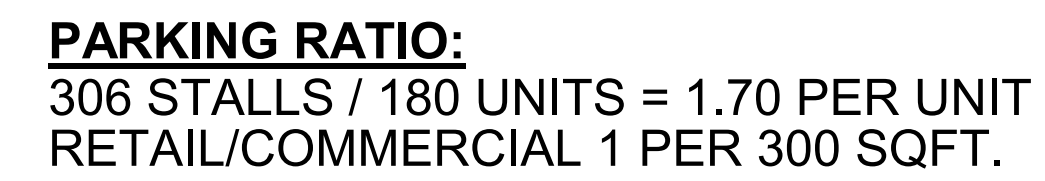
## REFERENCES

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## ATTACHMENT 1

### SITE PLANS

*SUBJECT TO CHANGE*



TRUE  
NORTH

1" = 50'-0"

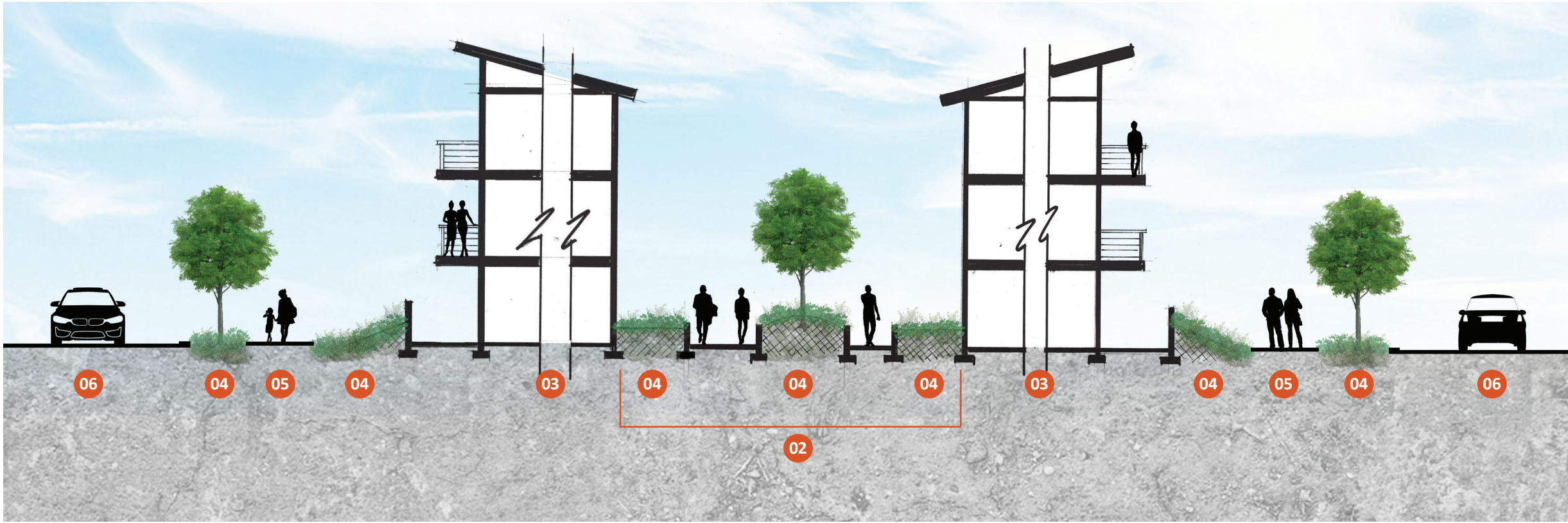
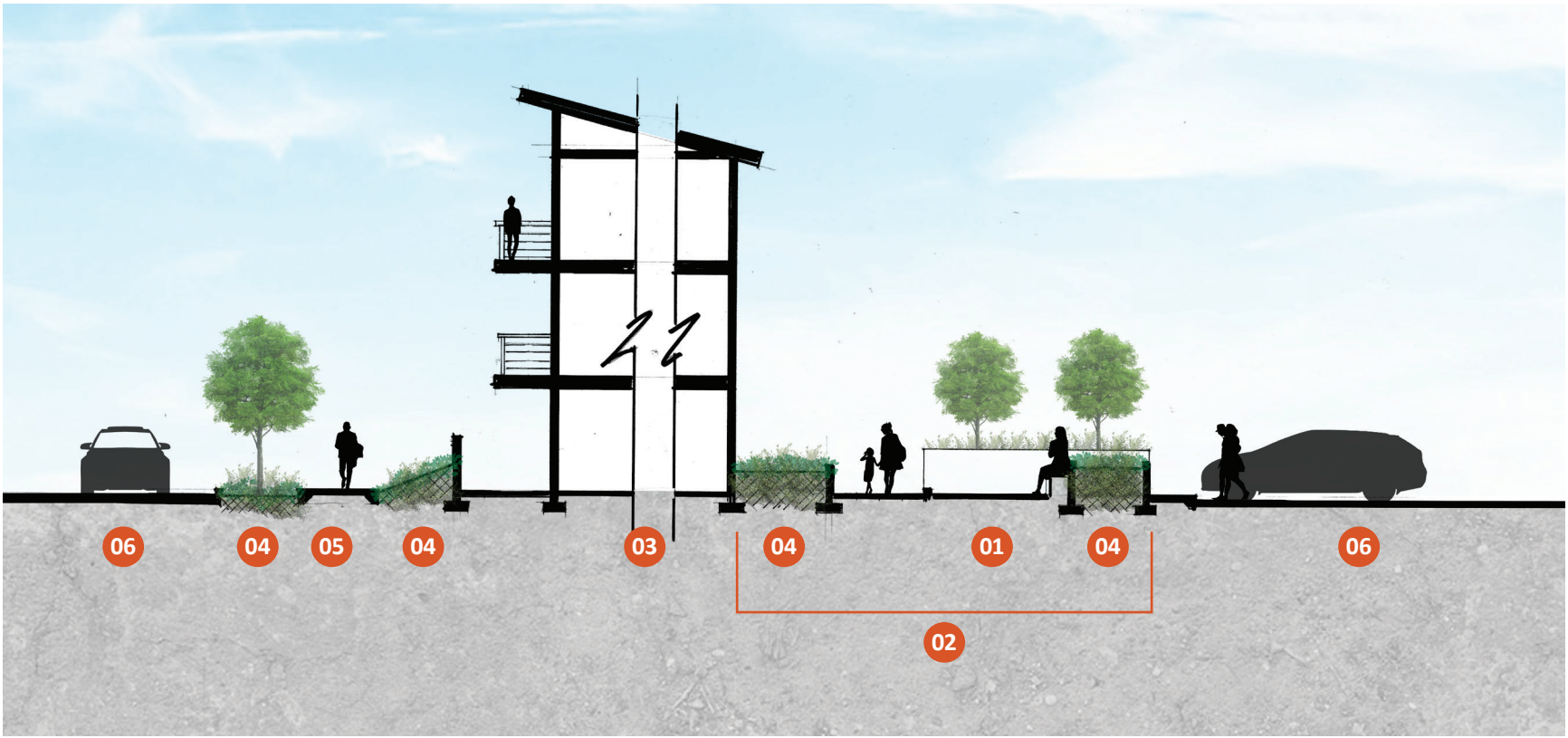


# EXHIBIT B: Conceptual Sections

DESIGN

## Legend

- 01 Bocce Ball Court
- 02 Linear Park
- 03 2 Bedroom Unit
- 04 Planter/Landscape
- 05 Sidewalk
- 06 Drive lane/parking





This aerial map illustrates the Chain Lake area in Monroe, Louisiana. The proposed development site is highlighted with a white outline and a grey road network. Surrounding the site are various commercial establishments, including The UPS Store, Kenneth C. Lin MD at Everett Bone & Joint, Lowe's Garden Center, Lowe's Home Improvement, Goodwill Monroe, Monroe Digital Copy Center, Chain Lake Self Storage, Emerald City Athletics, Sky Valley Sports, Best Western Sky Valley Inn, Kami Teriyaki & Sushi, O'Reilly Auto Parts, and Evergreen Inn & Suites, Monroe. The map also shows major roads such as N Kelsey St, Chain Lake Rd, and Tjerne Pl SE and E.



Exhibit D: Aerial with Site Plan Overlay



N. KELSEY ST.

N. KELSEY ST.

CHAIN LAKE RD.

## PROJECT DATA

SITE AREA:		±1,519,186 SF (34.88 ACRES)	
BUILDING AREA:		PARKING REQUIRED:	
RETAIL ANCHOR #1:	127,000 SF	• 1/ 250 SF=	532 STALLS
RETAIL ANCHOR #2:	125,400 SF	• 1/ 250 SF=	502
RETAIL ANCHOR #3:	60,000 SF	• 1/ 250 SF=	260
RETAIL SHOPS A:	13,150 SF	• 1/ 250 SF=	53
RETAIL SHOPS B:	8,500 SF	• 1/ 250 SF=	34
RETAIL SHOPS C:	9,205 SF	• 1/ 250 SF=	37
RESTUARANT PAD:	6,000 SF	• 1/ 100 SF=	60
TOTAL:	349,255 SF		1,418 STALLS
PARKING PROVIDED:		1,604 STALLS	
PARKING RATIO:		1/218 SF	

PRELIMINARY SITE PLAN  
SCALE: 1" = 60'

MONROE RETAIL  
SWC NORTH KELSEY STREET

MONROE, WASHINGTON

DONAHOU DESIGN GROUP ARCHITECTS, L.L.C.  
2150 NORTH 107TH ST. SUITE 300 SEATTLE, WASHINGTON 98133 TEL: (206)563-1960 FAX: (206)563-1768

PRELIMINARY SITE PLAN

SD-3

DATE: 6-23-03  
JOB #: 2003-10  
FILE NAME: altsite.DWG



## MEMORANDUM

Date: June 27, 2018  
To: Deborah Knight and Ben Swanson, City of Monroe  
From: Sarah Peters and Kendra Breiland  
Subject: **N. Kelsey Sub-Area Trip Generation Analysis**

*PT18-0011*

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In 2004 the City of Monroe adopted the North Kelsey Sub-Area Plan and the associated North Kelsey Supplemental Environmental Impact Statement (SEIS). The plan approved a land use program that includes a mix of retail, office, and housing. Since 2004, large format retail, a shopping center,, a clinic and medical offices, and a fast food restaurant have been constructed in the North Kelsey area. The City is currently considering a land use proposal to construct 200 units of multifamily housing accompanied by 54,022 square feet (sf) of retail. To understand whether the proposed development would create traffic impacts and require mitigations beyond what was foreseen in the SEIS, this memorandum compares afternoon peak hour trip generation for land uses approved in 2004 to trip generation for the already-constructed and proposed land uses.

### Land Uses

The North Kelsey Sub-Area is a 68-acre site located north of US Highway 2 in the City of Monroe, generally extending along North Kelsey Street (north of Tjerne Place SE) and along Galaxy Way. Adopted in 2004, the North Kelsey Sub-Area Plan SEIS identified land uses, infrastructure needs, and design standards to guide the development of the Sub-Area. Since 2004, new construction has used some of the development capacity defined in the SEIS. Approved, already-constructed, and proposed land uses are described in **Table 1** and discussed below.





Table 1: Land Use Scenarios				
Scenario		Land Use Summary	Size	Unit
1	Action Alternative 1 (2014 Full Build-out) <sup>1</sup>	Office	100	KSF
		Retail	500	KSF
		Multifamily Housing	150	DU
		Community Center	37.75	KSF
		Lakeside Industries (relocation)	38	employees
2	Constructed since 2004 <sup>2</sup>	Retail/Services: Fast Food (Carl's Junior)	2.97	KSF
		Retail: Discount Super Store (Walmart)	156.82	KSF
		Retail: Hardware/Paint Store (Lowe's)	139.41	KSF
		Retail: Shopping Center (Kelsey Road Station)	21.02	KSF
		Retail Subtotal	320.22	KSF
		Medical: Clinic (Providence Medical)	41.90	KSF
3	Proposed development <sup>3</sup>	Retail	54.02	KSF
		Multifamily Housing	200	DU

1. Source: North Kelsey Sub Area Planned Action Final SEIS, March 20, 2004.

2. Source: City of Monroe staff via email, June 2018. Two currently operating uses, Galaxy Theater and the Lakeside Industries asphalt plant, were already operating in 2004.

3. Source: City of Monroe staff via email, June 2018.

KSF=thousand square feet; DU=dwelling units.

## Adopted Land Uses

The City of Monroe adopted Action Alternative 1 (2014 Full Buildout) described in the North Kelsey Sub-Area SEIS, which includes 100,000 sf of office space, 500,000 sf of retail space, 150 multifamily housing units, and a 37,750 sf community center. The SEIS also analyzed the relocation of the Lakeside Industries asphalt plant within the Sub-Area, which would not add any new trips.

## Already-Constructed Land Uses

Several commercial sites have been developed since the SEIS was adopted in 2004. A total of 320,220 sf of retail space, including a Carl's Junior fast food restaurant, a Walmart discount store, a Lowe's hardware store, and the Kelsey Road Station shopping center have been built and are currently operating. The Providence Medical Clinic, totaling 41,986 sf, provides urgent care and family and internal medicine appointments.

## Proposed Land Uses

The current development proposal includes 200 multifamily housing units and 54,022 sf of retail space.



## Trip Generation

To assess whether the proposed development would create traffic impacts and require mitigations beyond what was evaluated in the SEIS, afternoon peak hour trip generation estimates were prepared for three scenarios: the land uses adopted in the SEIS, already-constructed land uses, and the current proposed land uses.

### Methods

Vehicle trip generation estimates were prepared using two approaches:

- 1) Trip generation estimates using rates from the Institute of Transportation Engineers *Trip Generation* (9<sup>th</sup> Edition)<sup>1</sup>
- 2) Trip generation estimates adjusted using MainStreet, a tool developed by Fehr & Peers that adjusts ITE trip generation estimates to account for urban context and the interactions between land uses.

Trip generation rates published by ITE have been used to estimate the impacts of development for decades and are commonly used as a first step in travel demand modeling. When conducting a stand-alone analysis, however, ITE rates tend to overestimate trip generation for developed areas that combine different land uses. The MainStreet/MXD+ trip generation method combines two trip estimation approaches (the NCHRP 684 method and the EPA MXD method) to assess the traffic impacts of mixed use development within a specific transportation context. This approach reduces vehicle trip generation estimates to account for vehicle trips made within a single area (internal trips) and trips made by walking, bicycling, or transit.

In most places, the afternoon peak hour has the highest level of traffic congestion during the day and sees the greatest level of traffic impacts. This is true in Monroe, where traffic congestion is considerably higher during the afternoon than during the morning. Consistent with transportation planning practice and per direction of City of Monroe staff, this analysis uses PM peak hour trip generation to assess the remaining development capacity.

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<sup>1</sup> A 10<sup>th</sup> Edition of *Trip Generation*, published in 2017, includes substantial changes to trip rates for several employment land uses. Rates from the 9<sup>th</sup> Edition were used to provide consistency with the analysis approach that would have been used in the 2004 SEIS to assess impacts and infrastructure needs for the Sub-Area.



## Results

Trip generation estimates for the three development scenarios are shown below and in **Table 2**.

- 1) Action Alternative 1 (Full Buildout):** The adopted land use alternative would generate approximately 2,025 to 2,200 trips during the afternoon peak hour. The high estimate (ITE) is based on standard trip generation methods; the low estimate (MXD+) accounts for trips made between land uses in the Sub-Area. Trip estimates for Action Alternative 1 define the upper limit of total trip generation that can occur in in the Sub-Area while remaining consistent with the 2004 SEIS.
- 2) Constructed since 2004:** Already-constructed development in the Sub-Area is estimated to generate approximately 1,600 trips (MXD+) to 1,750 trips (ITE). Subtracting this from the estimates for Action Alternative 1 yields a remaining capacity of 400 to 450 trips.
- 3) Proposed development:** The proposed development would generate approximately 300 to 325 trips during the PM peak hour, falling within the remaining capacity defined by Action Alternative 1 and the already-constructed uses.

<b>Table 2: PM Peak Hour Vehicle Trips by Land Use Scenario</b>				
<b>Land Use</b>		<b>Land Use Summary</b>	<b>ITE<sup>4</sup></b>	<b>MXD+<sup>5</sup></b>
<b>1</b>	<b>Action Alternative 1 (2014 Full Build-out)<sup>1</sup></b>	Office: 100 KSF Retail: 500 KSF Multifamily Housing: 150 units Community Center: 37.75 KSF	2,195	2,021
<b>2</b>	<b>Constructed since 2004<sup>2</sup></b>	Fast Food (Carl's Junior): 2.97 KSF Discount Super Store (Walmart): 156.82 KSF Hardware/Paint Store (Lowe's): 139.41 KSF Shopping Center (Kelsey Road Station): 21.02 KSF Clinic (Providence Medical): 41.896 KSF	1,749	1,606
<b>Remaining Trip Capacity (1-2)</b>			<b>446</b>	<b>415</b>
<b>3</b>	<b>Proposed development<sup>3</sup></b>	Shopping Center: 54 KSF Apartment: 200 units	324	294
<b>Remaining Trip Capacity (1-2-3)</b>			<b>122</b>	<b>121</b>

1. Source: North Kelsey Sub Area Planned Action Final SEIS, March 20, 2004.

2. Source: City of Monroe staff via email, June 2018. Does not include Galaxy Theater or Lakeside Industries asphalt plant, which were already operating in 2004.

3. Source: City of Monroe staff via email, June 2018.

4. Calculated using average trip rates for peak hour of adjacent streets, ITE Trip Generation, 9th Edition.

5. Calculated using MainStreet trip generation tool. Accounts for trip internalization; does not account for pass-by trip reductions.



## **Next Steps for Analysis**

Taken together with development that has been constructed since 2004, the proposed development would not generate more afternoon peak hour trips than were evaluated in the 2004 SEIS. Based on this assessment, there is no need to conduct additional analysis to identify traffic impacts and mitigations.

However, the development that has been constructed since 2004 differs somewhat from what was evaluated in the SEIS. While the overall number of trips may be approximately the same, the routes they take may differ from what was originally evaluated. Given that, the City may want to reexamine the mitigations originally called for in the SEIS and develop updated operational forecasts based on an updated trip distribution. These forecasts could be used to identify new mitigation approaches for any locations where improvements have not yet been constructed.





## **Attachments**

**Attachment A: Trip generation results tables**

**Attachment B: Notes from check-in calls with City staff**



## **Attachment A: Trip generation results tables**

Alternative A

Land Use	Units1	ITE Code	Quantity	PM Trips		
				In	Out	Total
<b>Net New Uses</b>						
(710) - General Office Building (Pk Hr, AM & PM)	1000 sq ft gross floor area	7102	100	25	124	149
(820) - Shopping Center (Adj Streets, 7-9A, 4-6P)	1000 sq ft leasable area	8203	500	890	965	1,855
(220) - Apartment (Adj Streets, 7-9A, 4-6P)	Dwelling Units	2204	150	60	33	93
(495) - Recreational Community Center (Adj Streets, 7-9A, 4-6P)	1000 sq ft gross floor area	4955	35.75	48	50	98
<b>Net Raw Project Trips</b>				<b>1,023</b>	<b>1,172</b>	<b>2,195</b>
<b>Reductions</b>						
Internal Capture				-21	-25	-46
External Walk, Bike, and Transit				-60	-68	-128
Internal Capture and Walk/Bike/Transit Trips				-81	-93	-174
<b>Net New Project Trips</b>				<b>942</b>	<b>1,079</b>	<b>2,021</b>

# 2004-2018 Growth

Land Use	Units	ITE Code	Quantity	PM Trips		
				In	Out	Total
<b>Net New Uses</b>						
(934) - Fast-Food with Drive-Through Window (Adj Streets, 7-9A, 4-6P)	1000 sq ft gross floor area	9342	2.971	50	47	97
(813) - Free Standing Discount Super Store (Adj streets, 7-9A, 4-6P)	1000 sq ft gross floor area	8133	156.82	334	348	682
(816) - Hardware/Paint Store (Adj Streets, 7-9A, 4-6P)	1000 sq ft gross floor area	8164	139.412	317	358	675
(820) - Shopping Center (Adj Streets, 7-9A, 4-6P)	1000 sq ft leasable area	8205	21.018	37	41	78
(630) - Clinic (Adj Streets, 7-9A 4-6P)	1000 sq ft gross floor area	6306	41.896	89	128	217
<b>Net Raw Project Trips</b>				<b>827</b>	<b>922</b>	<b>1,749</b>
<b>Reductions</b>						
Internal Capture and Walk/Bike/Transit Trips				-67	-76	-143
<b>Net New Project Trips</b>				<b>760</b>	<b>846</b>	<b>1,606</b>

Proposed Development

Land Use	Units	ITE Code	Quantity	PM Trips		
				In	Out	Total
<b>Net New Uses</b>						
(220) - Apartment (Adj Streets, 7-9A, 4-6P)	Dwelling Units	2202	200	81	43	124
(820) - Shopping Center (Adj Streets, 7-9A, 4-6P)	1000 sq ft leasable area	8203	54.022	96	104	200
<b>Net Raw Project Trips</b>				<b>177</b>	<b>147</b>	<b>324</b>
<b>Reductions</b>						
Internal Capture and Walk/Bike/Transit Trips				-17	-13	-30
<b>Net New Project Trips</b>				<b>160</b>	<b>134</b>	<b>294</b>



## **Attachment B: Notes from calls with City staff**



### **Check-in call – June 13, 2018**

City of Monroe: Ben Swanson

Fehr & Peers: Sarah Peters

Direction from City staff in **bold text**.

Question: What are the current proposed land uses?

**Response: Allow mixed use: co-locate MFH and commercial. Ben to re-send site plan.**

Question: Which alternative should we compared the proposed land uses to (i.e. which of the SEIS alternatives was adopted)?

- Action Alternative 1 - 2014 Full Build-out
- Action Alternative 2 - 2008 Limited Growth

**Response: Ben will provide direction. [Note: Per email received 6/18/2018, Action Alternative 1 was selected.]**

Question: Can you point us to the appendices for the N. Kelsey SEIS? The transportation assessment is the most critical item. This will help us refine the trip generation estimates for the adopted alternative and existing land uses.

**Response: Refer to City of Monroe Traffic Improvement Study (Entranco, 2004), which used the same information and analysis approach as the SEIS. [Note: Fehr & Peers received copy of study via overnight mail on June 18, 2018.]**

Question: What has already been constructed and/or approved within the sub-area? So far we have received TIAs for the following:

- 155,845 retail store at N. Kelsey St./Galaxy Way
- ~~35 single family homes (Leighty Estates)~~
- ~~25 single family homes (Raspberry Hill)~~

**Response: Include Wal-Mart at N. Kelsey and Galaxy, Loew's, and Providence Medical. Do not include housing – these developments fall outside the Sub-Area. Ben to send list of buildings developed since 2004. [Note: Fehr & Peers received list of developments in sub-area via email on 6/18/2018. See table below.]**

The following are a list of structures in the sub-area:

Business Name	Square feet
Carl's Jr	2,971
Walmart	156,820
Lowe's	139,412
Kelsey Road Station (mini mall)	21,018
Galaxy Movie Theater	61,020
Lakeside Ind. (Asphalt manufacturing)	2,534
Providence Medical	41,896

**General Direction: North Kelsey Plan Sub-Area – may need to differentiate public and privately owned land and associated development envelopes. Ben to provide direction.**



## **Initial results review – June 19, 2018**

City of Monroe: Ben Swanson

Fehr & Peers: Sarah Peters

Direction from City staff in **bold text**.

- Initial assessment shows that adequate development envelope exists for proposed development. (See table below.)
- Based on the technical approach used in 2004 Entranco study, the trip generation for the 2004 SEIS was likely prepared using a travel demand model. To provide a consistent analysis, Fehr & Peers will use ITE trip rates/MainStreet tool to evaluate all land use scenarios.
- Priority time period is PM peak hour – this is when impacts would be identified given existing traffic patterns in Monroe.

Land Use	Land Use Summary	Vehicle Trips**		
		Daily	AM	PM
Action Alternative 1 (2014 Full Build-out)*	Office: 100 KSF Retail: 500 KSF Multifamily Housing: 150 units Community Center: 37.75 KSF	23,305	699	2,021
Constructed since 2004 (does not include Galaxy Theater or Lakeside Industries asphalt plant)	Fast Food (Carl's Junior): 2.97 KSF Discount Super Store (Walmart): 156.82 KSF Hardware/Paint Store (Lowe's): 139.41 KSF Shopping Center (Kelsey Road Station): 21.02 KSF Clinic (Providence Medical): 41.896 KSF	17,998	539	1,606
<i>Remaining Capacity</i>		5,307	160	415
Proposed development	Retail: 54 KSF Multifamily Housing: 200 units	3407	137	294
<i>Remaining Capacity</i>		1,900	23	121
* Source: North Kelsey Sub Area Planned Action Final SEIS, March 20, 2004				
** Estimated using MainStreet trip generation tool. Accounts for trip internalization; does not account for pass-by trip reductions.				